

Our ref: SSI-10038-PA-663

[REDACTED]
Director Environment, Sustainability & Planning
Sydney Metro

Attention: [REDACTED] – Manager Environment

1/10/2024

Subject: Sydney Metro West, Central Tunnelling Package – Construction Parking and Access Strategy, Revision 10, dated 16 September 2024

Thank you for submitting the Construction Parking and Access Strategy, Revision 10, dated 16 September 2024 for Sydney Metro West, Central Tunnelling Package (the document) on 18 September 2024.

I note the CPAS:

- addresses changes to worker parking at Five Dock West and Burwood North for the demobilisation phase; and
- has been reviewed by Sydney Metro and no issues have been raised with the Department;

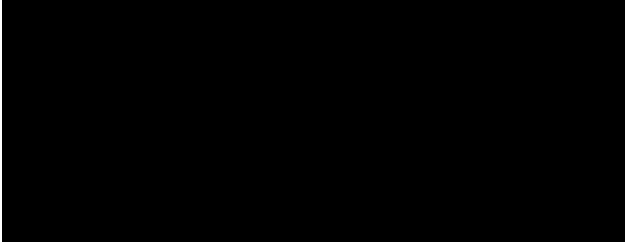
Accordingly, I approve the Construction Parking and Access Strategy, Revision 10, dated 16 September 2024 under Condition D91 of SSI 10038, as nominee of the Planning Secretary.

If there are any inconsistencies between the document and the conditions of approval, the conditions prevail.

Please make the document publicly available on the project website as soon as possible.

If you wish to discuss the matter further, please contact [REDACTED] at [REDACTED]

Yours sincerely



As nominee of the Planning Secretary



Construction Parking and Access Strategy

SMWSTCTP AFJ-1NL TF PLN 00003 Rev 10

Sydney Metro West Central Tunnelling Package



DOCUMENT APPROVAL

	Prepared By	Reviewed By	Approved By
Name:			
Position:			
Date:			

REVISION HISTORY

Rev:	Date:	Pages:	By:	Description:
A	02/11/2021	ALL		For internal review
00	23/11/2021	ALL		For submission to Sydney Metro
01	15/12/2021	ALL		For approval to Sydney Metro and DPIE
02	24/02/2023	ALL		For reapproval to Sydney Metro and DPIE
03	02/05/2023	ALL		For reapproval to Sydney Metro and DPIE
04	04/10/2023	ALL		For reapproval to Sydney Metro and DPE
05	27/11/2023	ALL		For reapproval to Sydney Metro and DPE and to address DPE RFI
06	14/12/2023	ALL		For reapproval to Sydney Metro and DPE
07	09/02/2024	ALL		For reapproval to Sydney Metro and DPE
08	14/03/2024	ALL		Updated to address DPHI RFI issued 29/02/2024 and for reapproval to Sydney Metro and DPHI.
09	31/05/2024	ALL		Updated parking arrangements at The Bays, for reapproval to Sydney Metro and DPHI.
10	16/09/2024	ALL	Updated parking arrangements at Burwood and Five Dock for demobilisation. For reapproval to Sydney Metro and DPHI	

CONTENTS

DOCUMENT APPROVAL	1
REVISION HISTORY	1
CONTENTS	2
1. INTRODUCTION	1
2. PURPOSE AND SCOPE	2
3. WORK SITES & PARKING FACILITIES	5
3.1 WORK SITES	5
3 2 WORKER PARKING AVAILABILITY AND DEMAND	5
3.3 THE BAYS	7
3 3 1 SITE INFORMATION	7
3.3.2 PARKING DEMAND.....	7
3.4 FIVE DOCK	7
3.4.1 SITE INFORMATION	7
3 4 2 PARKING DEMAND	8
3.5 BURWOOD NORTH	9
3 5 1 SITE INFORMATION	9
3.5.2 PARKING DEMAND.....	9
3.6 NORTH STRATHFIELD	11
3.6.1 SITE INFORMATION	11
3 6 2 PARKING DEMAND	11
3.7 SYDNEY OLYMPIC PARK	12
3.7.1 SITE INFORMATION	12
3.7.2 PARKING DEMAND.....	12
4. PARKING REMOVAL	14
4.1 THE BAYS	14
4.2 FIVE DOCK	14
4.2.1 EASTERN CONSTRUCTION SITE	14
4 2 1 1 Justification for exceeding the EIS identified parking removal	15
4.2.2 WESTERN CONSTRUCTION SITE	15
4.2.2.2 Justification for exceeding the EIS identified parking removal	16
4.3 BURWOOD NORTH	16
4.3.2.1 Justification for exceeding the EIS identified parking removal	17
4.4 NORTH STRATHFIELD	17
4.4.2.1 Justification for exceeding the EIS identified parking removal	18
4.5 SYDNEY OLYMPIC PARK	18
4.6 PARKING AVAILABILITY	18
4.6.3 PARKING SUPPLY	19
4 6.4 PARKING SURVEY RESULTS	24

4.6.4.1 The Bays	24
4.6.4.2 Five Dock	25
4.6.4.3 Burwood North	31
4.6.4.4 North Strathfield	37
4.6.4.5 Sydney Olympic Park	39
4.6.5 SPECIAL EVENTS	40
4.7 CONCLUSION OF THE PARKING DEMAND SURVEY	40
4.7.1 THE BAYS	40
4.7.2 FIVE DOCK	40
4.7.3 BURWOOD NORTH	40
4.7.4 NORTH STRATHFIELD	41
4.7.5 SYDNEY OLYMPIC PARK	41
4.8 MITIGATION MEASURES	41
4.8.1 MINIMISE PARKING ON PUBLIC ROADS	41
4.8.2 AFJV SHUTTLE BUSES	44
4.8.2.1 AFJV bus service - The Bays	44
4.8.2.2 AFJV bus service - Sydney Olympic Park	44
4.8.2.3 AFJV bus service - routes and stops	44
4.8.3 ADDITIONAL MITIGATION MEASURES UNDER CONSIDERATION	44
4.8.4 CONTINGENCY MEASURES	45
4.8.4.4 The Bays	45
4.8.4.5 Five Dock	45
4.8.4.6 Burwood North	45
4.8.4.7 North Strathfield	45
4.8.4.8 Sydney Olympic park	45
4.8.5 MINIMISE IDLING AND QUEUING ON STATE AND REGIONAL ROADS	45
4.8.6 MARSHALLING OF CONSTRUCTION VEHICLES	45
4.8.6.1 The Bays	46
4.8.7 OTHER MITIGATION MEASURES	46
4.9 MONITORING	47
4.9.1 INSPECTION	47
4.9.2 REPORTING	48
4.10 CONTINGENCY	48
4.10.1 ADDITIONAL OVERFLOW PARKING	48
4.10.2 RE-EDUCATION AND CORRECTION	48
4.10.3 SHIFT STRUCTURES	48
4.11 ACCESS CONTROL AND SAFETY	48
4.11.1 PEDESTRIAN INTERFACE WITH DRIVEWAYS	48
4.11.2 MANAGING PEDESTRIANS	49
4.11.3 MANAGING CYCLIST	50

4.11.4 ACCESS TO RESIDENTS AND BUSINESS	50
4.11.5 EMERGENCY ACCESS.....	50
4.11.6 CONSTRUCTION SITE ACCESS	51
4.11.7 TRAFFIC IMPACT OF CONSTRUCTION ACTIVITIES.....	51
4.12 CONSULTATION	52
4.12.8 THE BAYS.....	52
4.12.9 FIVE DOCK	52
4.12.10 BURWOOD NORTH.....	53
4.12.11 NORTH STRATHFIELD	53
4.12.12 SYDNEY OLYMPIC PARK.....	54
5. CONCLUSION	55
6. APPENDICES	56
6.1 PARKING SURVEY SUMMARY	56
6.2 SAMPLE HEAVY VEHICLE ROUTES (VEHICLE MOVEMENT PLANS)	57
6.3 WORKER PARKING MAPS	58
6.4 HEAVY VEHICLE TURN PATHS – MARSHALLING AREAS	59
6.5 BUS SERVICES VEHICLE MOVEMENT PLANS (VMP)	60

1. INTRODUCTION

Sydney Metro is Australia's biggest public transport program. Services on the North West Metro Line between Rouse Hill and Chatswood started in May 2019. The Sydney Metro network also includes Sydney Metro City & Southwest, Sydney Metro West and Sydney Metro Western Sydney Airport

Sydney Metro West is a new 24 kilometre metro line between Westmead and the Sydney CBD (the Project) This infrastructure investment will double the rail capacity of the Greater Parramatta to Sydney CBD corridor with a travel time target between the two centres of about 20 minutes

The planning approvals and environmental impact assessment for Sydney Metro West has been split into a number of stages recognising the size of the project This includes:

- Stage 1 Concept and all major civil construction works including station excavation and tunnelling between Westmead and The Bays. Planning approval for this stage was granted in March 2021
- Stage 2 All major civil construction works including station excavation and tunnelling from The Bays to Sydney CBD
- Stage 3 Tunnel fit-out, construction of stations, ancillary facilities and station precincts, and operation and maintenance of the Sydney Metro West line

Due to the Project's importance, the Project was declared to be Critical State Significant Infrastructure by the Minister for Planning and Public Spaces An Environmental Impact Statement (EIS) (Jacobs/Arcadis, 2020) for the Concept and Stage 1 (herein referred to as the Project) was placed on public exhibition from 30 April 2020 to 26 June 2020. Submissions were received from government, agencies, organisations and the public in response to the project A Submissions Report was prepared by Sydney Metro in response to submissions received during the exhibition period and an Amendment Report was prepared by Sydney Metro in 2020 as a result of continued design development and refinement The Project was approved on 11 March 2021 (SSI 10038)

Stage 1 of the Project is being delivered under a number of packages in accordance with the Sydney Metro West Phasing Report The packages include:

- Phase A Power Enabling Works
- Phase B1 Central Tunnelling Package (Civils Works)
- Phase B2 Central Tunnelling Package (Tunnelling Works)
- Phase C Parramatta, Westmead and Clyde Demolition Works
- Phase D Greater Sydney Road Works
- Phase E Existing Rail Corridor Enabling Works
- Phase F Western Tunnelling

The Central Tunnelling Package (CTP) involves the design and construction of 11km of twin tunnels and underground station excavations from The Bays to Sydney Olympic Park.

This document has been prepared for the CTP to identify and mitigate impacts resulting from on- and off street parking changes during construction as noted in Condition D90 of the Project Conditions of Approval (CoA) for Sydney Metro West

2. PURPOSE AND SCOPE

This document has been prepared to address the Sydney Metro West Central Tunnelling Package (CTP) project Condition of Approval as shown in Table 1, which includes a cross reference to where the information has been addressed within this document

TABLE 1 COMPLIANCE MATRIX

SYDNEY METRO WEST CONCEPT AND STAGE 1 CONDITIONS OF APPROVAL		
Reference		Where addressed
D90	Vehicles associated with the project workforce (including light vehicles and Heavy Vehicles) must be managed to:	This document
	(a) minimise parking on public roads;	Section 4.8.1
	(b) minimise idling and queueing on state and regional roads;	Section 4.8.5
	(c) not carry out marshalling of construction vehicles near sensitive land user(s);	Section 4.8.6
	(d) not block or disrupt access across pedestrian or shared user paths at any time unless alternate access is provided; and	Section 4.11
	(e) ensure spoil haulage vehicles adhere to the nominated haulage routes identified in the CTMPs.	Section 4.9.1
D91	A Construction Parking and Access Strategy must be prepared to identify and mitigate impacts resulting from on and off-street parking changes during construction. The Construction Parking and Access Strategy must include, but not necessarily be limited to:	This document
	(a) achieving the requirements of Condition D90 above;	(a) Section 4.8
	(b) confirmation and timing of the removal of on and off-street parking associated with construction of Stage 1 of the CSSI;	(b) Section 4
	(c) parking surveys of all parking spaces to be removed or occupied by the project workforce to determine current demand during peak, off peak, school drop off and pickup, weekend periods and during special events;	(c) Section 4.6
	(d) consultation with affected stakeholders utilising existing on and off-street parking stock which will be impacted as a result of construction;	(d) Section 4.12
	(e) assessment of the impacts to on and off street parking stock taking into consideration, occupation by the project workforce, outcomes of consultation with affected stakeholders and considering the impacts of special events;	(e) Section 4.7, Section 4.12
	(f) identification of reasonable and practicable mitigation measures to manage impacts to stakeholders as a result of on and off street parking changes including, but not necessarily limited to, staged removal and replacement of parking, provision of alternative parking arrangements, managed staff parking arrangements and working with relevant council(s) to introduce parking restrictions adjacent	(f) Section 4.8

SYDNEY METRO WEST CONCEPT AND STAGE 1 CONDITIONS OF APPROVAL

Reference	Where addressed	
<p>to work sites and compounds or appropriate residential parking schemes;</p> <p>(g) where residential parking schemes already exist, off road parking facilities must be provided for the project workforce;</p> <p>(h) mechanisms for monitoring, over appropriate intervals (not less than 6 months), to determine the effectiveness of implemented mitigation measures;</p> <p>(i) details of shuttle bus service(s) to transport the project workforce to construction sites from public transport hubs and off-site car parking facilities (where these are provided) and between construction sites;</p> <p>(j) provision of contingency measures should the results of mitigation or monitoring indicate implemented measures are ineffective; and</p> <p>(k) provision of reporting of monitoring results to the Planning Secretary and Relevant Council(s) at six (6) monthly intervals.</p>	<p>(g) Section 4.8</p> <p>(h) Section 4.9.1</p> <p>(i) Section 4.8.2</p> <p>(j) Section 4.10</p> <p>(k) Section 4.9.2</p>	
D92	<p>The Construction Parking and Access Strategy must be submitted to the Planning Secretary for approval at least one (1) month before the commencement of any construction that reduces the availability of existing parking. The approved Construction Parking and Access Strategy must be implemented before impacting on on-street parking and incorporated into the CTMPs.</p>	This document
D93	<p>During construction, all reasonably practicable measures must be implemented to maintain pedestrian, cyclist and vehicular access to, and parking in the vicinity of, businesses and affected properties. Disruptions are to be avoided, and where avoidance is not possible, minimised.</p> <p>Where disruption cannot be minimised, alternative pedestrian, cyclist and vehicular access, and parking arrangements must be developed in consultation with affected businesses and implemented before the disruption. Adequate signage and directions to businesses must be provided before, and for the duration of, any disruption.</p>	This document Section 4.11

EIS TECHNICAL PAPER 1 STAGE 1 TRAFFIC AND TRANSPORT MITIGATION MEASURES

Reference		Where addressed
TT3	Access to properties for emergency vehicles would be provided at all times	Section 4.11.5
TT4	Vehicle access to and from construction sites would be managed to maintain pedestrian, cyclist and motorist safety. Depending on the location, this may require manual supervision, physical barriers, temporary traffic signals and modifications to existing signals or, on occasions, police presence.	Section 4.11
TT6	All trucks would enter and exit construction sites in a forward direction, where feasible and reasonable.	Section 4.11.6
TT8	Construction site traffic immediately around construction sites would be managed to minimise vehicle movements through school zones during pick up and drop off times.	Section 4.11.7
TT10	Where existing parking is removed to facilitate construction activities, consultation would occur with the relevant local council to investigate opportunities to provide alternative parking facilities.	Section 4.11.7
TT11	Construction sites would be managed to minimise the number of construction workers parking on surrounding streets by: <ul style="list-style-type: none"> ▪ Encouraging workers to use public or active transport ▪ Encouraging ride sharing Provision of alternative parking locations and shuttle bus transfers where feasible and reasonable.	Section 4.8
TT18	Access to existing properties and buildings would be maintained in consultation with property owners.	Section 4.11.4
TT23	Opportunities to provide vehicle access and egress directly to Parramatta Road and minimise the use of Loftus Street at the Burwood North Station construction site would be explored during detailed design.	Section 4.11.6
TT31	Where existing parking is removed to facilitate construction activities for The Bays Station construction site power supply route, consultation would occur with the relevant local council, local businesses, the community and schools (where appropriate) to investigate opportunities to provide alternative parking facilities.	N/A There is no removal of parking around The Bays Station
TT32	Provision of assistance to carry shopping, luggage and other heavy or large goods between the alternative parking area at Ausgrid Rozelle sub-transmission substation (subject to final agreement between Sydney Metro and Ausgrid) and residences during times when access is limited.	N/A AFJV is not using this parking area

This Strategy will outline potential parking impacts related to construction of the Project, and the proposed mitigation measures to minimise parking and access impacts. In addition, the strategy sets out mechanisms to monitor effectiveness of the mitigation measures and provides contingency options if measures are found to be unsuccessful.

3. WORK SITES & PARKING FACILITIES

3.1 WORK SITES

AFJV will endeavour to maximise car parking facilities within its compounds and work areas in order to minimise parking on local roads and streets in accordance with CoA D90

At The Bays, parking will be available onsite with an additional parking area being secured with NSW Ports Authority.

At Five Dock, workers are intended to utilise public transport, shuttle bus service or park at nearby kerbside and on-street parking locations with low parking utilisation where parking on public roads cannot be entirely eliminated.

At Burwood North, workers are intended to utilise public transport, shuttle bus service, St Lukes car parking area, or to park at nearby kerbside and on street parking locations with low parking utilisation where parking on public roads cannot be entirely eliminated

At North Strathfield, on-site parking is available for some workers. Due to the proximity of the worksite to the North Strathfield train station, it's expected a large portion of workers will catch train services to and from the site daily, a shuttle bus service will also be provided.

At Sydney Olympic Park, on-site parking is available for some workers. Additional paid parking is available around Sydney Olympic Park.

In terms of overall on street parking impacts, the Project will have limited impacts on existing street parking allowances, as many of the parking spaces that are proposed to be removed are associated with driveway access and adjacent buildings that are now being used by the Project. An assumption of 5% of workforce travelling to worksite via alternative mode of transport (public transport, carpool, and bike) was used in the parking assessment for sites other than North Strathfield

AFJV has established the construction programs and scope of work at all CTP Worksites with the goal to minimise on street parking/parking on public roads in accordance with CoA D90(a) of the Planning Approval. Construction considers and supports this outcome as detailed extensively in Section 4.8.1 of this Plan.

3.2 WORKER PARKING AVAILABILITY AND DEMAND

Worker parking demand will vary throughout delivery of the project, from demolition and site establishment through to excavation, piling and finally tunnel boring. Table 2 outlines the indicative peak worker numbers and parking capacity at parking areas throughout the construction period

This table does not account for workers travelling via alternate means of transport including public transport, by foot or by bicycle or carpooling of workers, which would reduce staff and workforce parking demand at each location

AFJV's approach to parking and access is compliant with the requirements of CoA D90 that is, to minimise on street parking by the workforce.

It is further noted that at times, on street parking of work vehicles will be unavoidable, this may include parking in no stopping zones or other areas where parking restrictions may or may not apply. Where regulatory or advisory signage conflicts with this unavoidable worker parking, these areas will be controlled in accordance with Traffic Guidance Schemes and any permits and licences required

TABLE 2: WORKFORCE AND INDICATIVE ONSITE PARKING

Timing	The Bays		Five Dock		Burwood North		North Strathfield		Sydney Olympic Park	
	Indicative Workforce (Day/Night)	Indicative On-site Parking	Indicative Total Workforce(Day /Night)	Indicative Carparks On-site Parking	Indicative Total Workforce(Day /Night)	Indicative On-site Parking	Indicative Total Workforce	Indicative On-site Parking	Indicative Total Workforce	Indicative On-site Parking
2022 (1st half)	130	47	109/0	0	88/0	0	40	10	27	70
2022 (2nd half)	200	15	124/30	0	141/15	21	56	15	54	55
2023 (1st half)	186/80	7	116/63	0	101/32	21	50	15	70	55
2023 (2nd half)	186/80	7	110/57	0	126/32	21	30	15	54	55
2024 (1st half)	186/80	7	56/20	4	73/32	25	40	15	30	63
2024 (2nd half)	186/80	7	56/20	4	*73/32	**25	10	15	30	63
2025 (1st half)	20/20	7	56/20	0	60/30	0	10	15	45	63

Notes:

On-site parking availability noted above within Table 2 only represents parking spaces within the AFJV sites. This does not include additional parking areas outside of AFJV sites. Additional parking areas outside of the AFJV sites are as detailed within Section 3.

*Staff and workforce (combined total) to reduce to approximately 60 per day and 30 per night from late October 2024

**On-site worker parking to reduce to 15 spaces in early October 2024 and 0 spaces in late October 2024 to facilitate demobilisation activities.

3 3 THE BAYS

3 3 1 SITE INFORMATION

The Bays Station will be located between Glebe Island and White Bay Power Station. The station will have direct access to the future Bays Waterfront Promenade.

3 3 2 PARKING DEMAND

The Bays construction site accommodated parking for up to 15 light vehicles in early 2023 before the area was handed over to Sydney Metro’s ETP contractor on 12th May 2023. The site now accommodates up to 7 on site parking spaces.

With up to 150 workers expected on site at any one time, an additional parking area has been secured through consultation with Ports Authority. The area is located at Glebe Island and contains approximately 149 parking spaces for light vehicles and 8 spaces for heavy vehicles. The additional parking area was secured on 16th May 2022 and is dedicated to AFJV staff and workforce only. As of mid June 2024, Port Authority require this section of land to be handed back to make way for works associated with the Western Harbour Tunnel project (WHT). A revised agreement has been reached between Sydney Metro and the NSW Port Authority to secure a different section of land on Glebe Island that will be made available to ensure continued availability of off-street worker parking. The new section of land will have approximately 165 parking spaces for light vehicles.

It is noted that the new parking area will not be capable of supporting any truck marshalling, for this reason a new truck marshalling area has been sourced. This is further detailed within section 4.8.

Further information on mitigations and bus services can be found within Section 4.8.

FIGURE 1: THE BAYS



3 4 FIVE DOCK

3 4 1 SITE INFORMATION

The Five Dock construction site is located in the core of Five Dock local centre of Great North Road. The Five Dock construction site would comprise two separate sites, an east site and a west site on

either side of Great North Road. The east site is along Second Avenue and Waterview Street. The west site is along Great North road between Fred Kelly Place and Second Avenue.

3.4.2 PARKING DEMAND

During construction, workforce and staff numbers around Five Dock is not expected to exceed 124 during the day shift and 63 during night shift. Through consultation with council and site parking studies three primary methods have been adopted for minimising and reducing impact of workers parking on public roads:

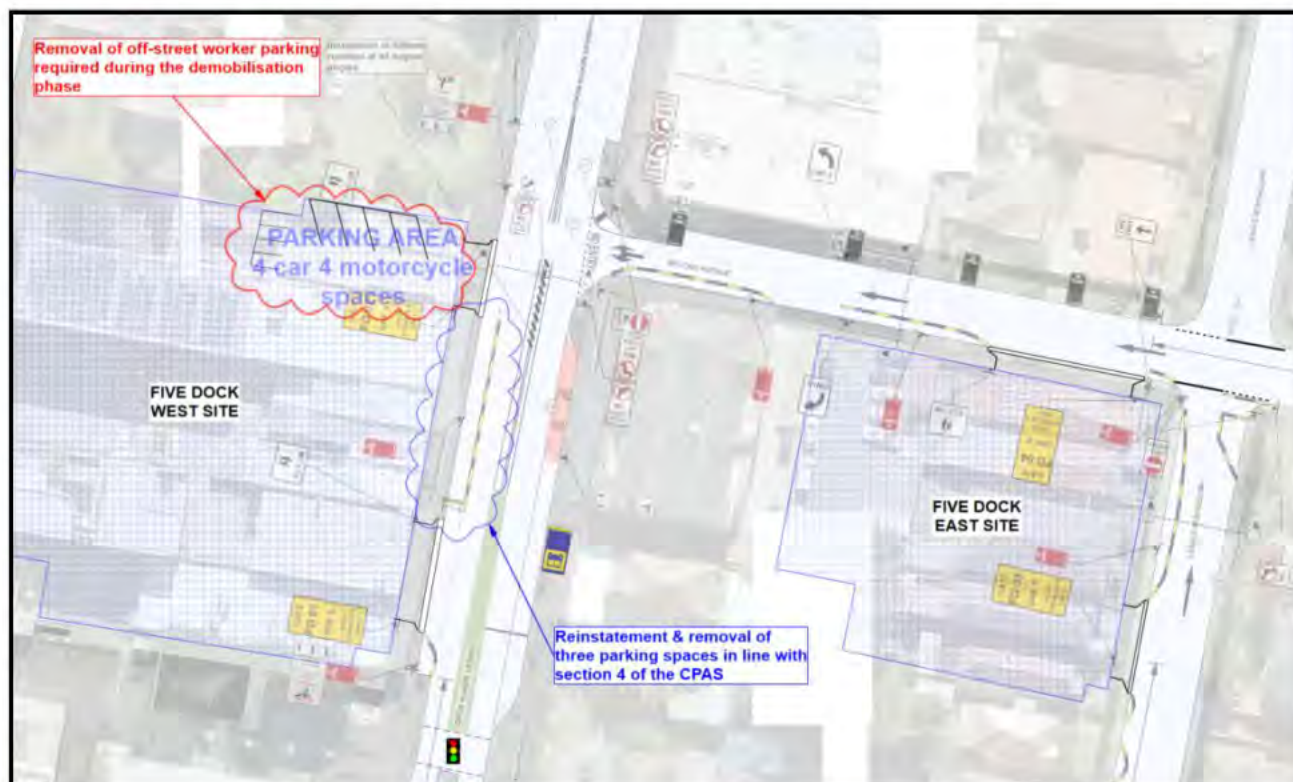
1. Continuation of shuttle bus services for workers from Sydney Olympic Park
2. Establishment of four (4) car and 4 motorcycle off street worker parking spaces at the northern end of the western site. These spaces are expected to become unavailable from approximately November 2024 due to site demobilisation activities. Before this occurs, AFJV will re-assess onsite staff and worker numbers with the aim to relocate staff to Sydney Olympic Park where practical
3. Continual encouragement of public transport use.

Where on street worker parking cannot be entirely eliminated by the primary methods listed above, the secondary option will be for workers to park on the 'Five Dock Park' side of Ingham Ave and Barnstaple Road and walk to site. The walk is only short, and workers behaviour requirements will be communicated and encouraged to keep noise to a minimum and avoid disruption to the residential areas

It is noted that this secondary strategy aims to reduce impact to the local community where workers parking on public roads cannot be eliminated entirely.

Further information on mitigations and bus services can be found within Section 4.8

FIGURE 2: FIVE DOCK



3.5 BURWOOD NORTH

3.5.1 SITE INFORMATION

The Burwood North construction site will comprise two separate sites, a North site and a South site on either side of Parramatta Road. The North site is along Parramatta Road between Burwood Road and Loftus Street. The South site is on the corner of Parramatta Road and Burwood Road.

3.5.2 PARKING DEMAND

Workforce and staff numbers around the Burwood North area are not expected to exceed 141 workers during day shift and 37 during night shift. During demobilisation from approximately October 2024 no worker parking will be available on-site. For this reason, workforce and staff numbers will be further reduced to minimise impact of worker parking.

Through consultation with council and site parking studies four primary methods have been adopted for minimising and reducing impact of workers parking on public roads:

1. Continuation of shuttle bus services for workers.
2. Provision of dedicated worker parking at the St Lukes Carpark (up to 40 spaces as agreed with CoCB Council)
3. Establishment of additional light vehicle and motorcycle parking spaces at both the northern and southern sites, totalling 25 car and 5 motorcycle worker parking spaces, as shown within Figure 3. Before these spaces become unavailable (approximately October 2024), re-assess onsite staff and worker numbers with the aim to relocate staff to Sydney Olympic Park where practical
4. Continual encouragement of public transport use.

Where on street worker parking cannot be entirely eliminated by the primary methods listed above, the secondary option will be for workers to park on Broughton Street and Addison Street, on the park sides. The walk is only short, and workers behaviour requirements will be communicated and encouraged to keep noise to a minimum and avoid disruption to the residential areas.

It is noted that this secondary strategy aims to reduce impact to the local community where workers parking on public roads cannot be eliminated entirely.

Further information on mitigations and bus services can be found within Section 4.8

FIGURE 3: BURWOOD NORTH

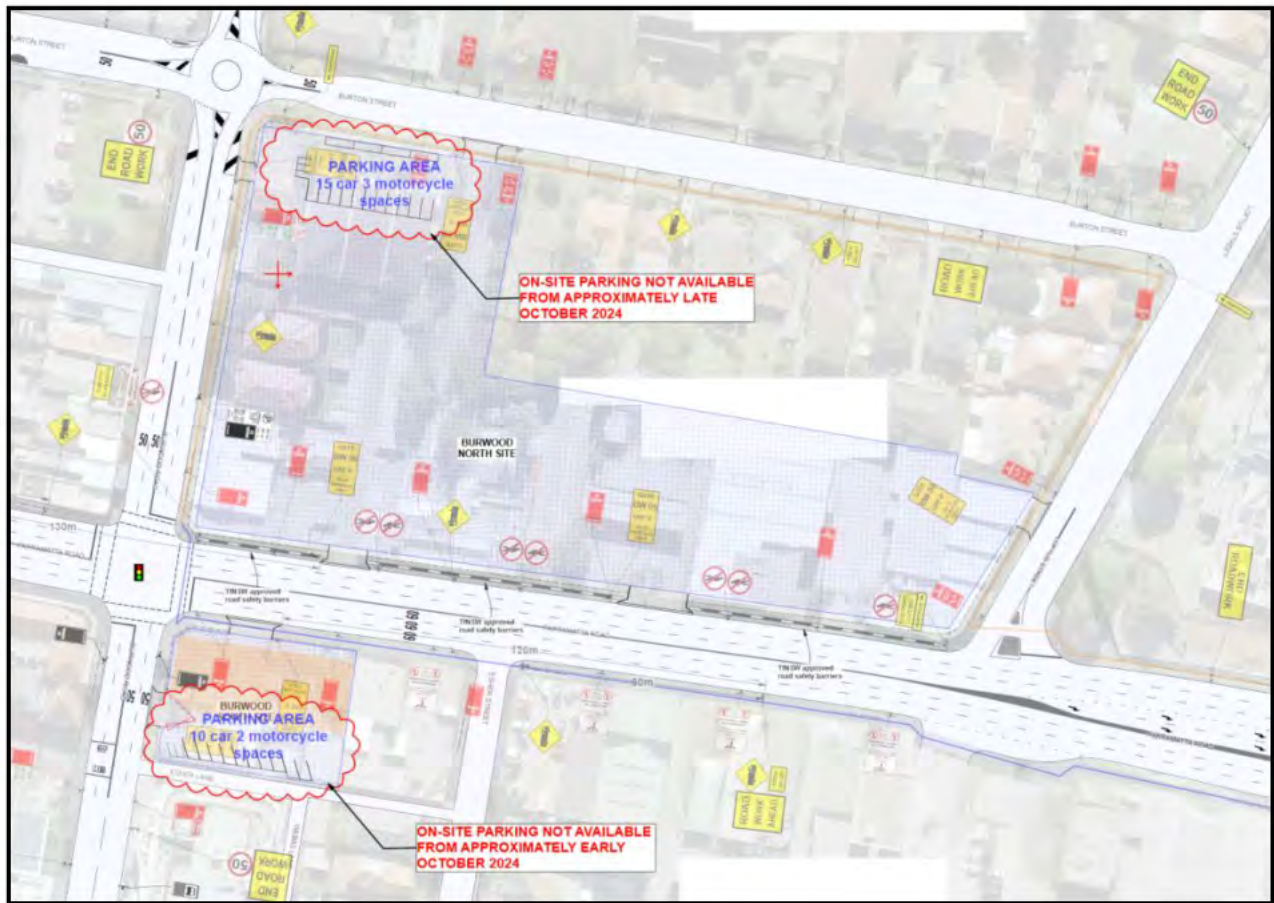
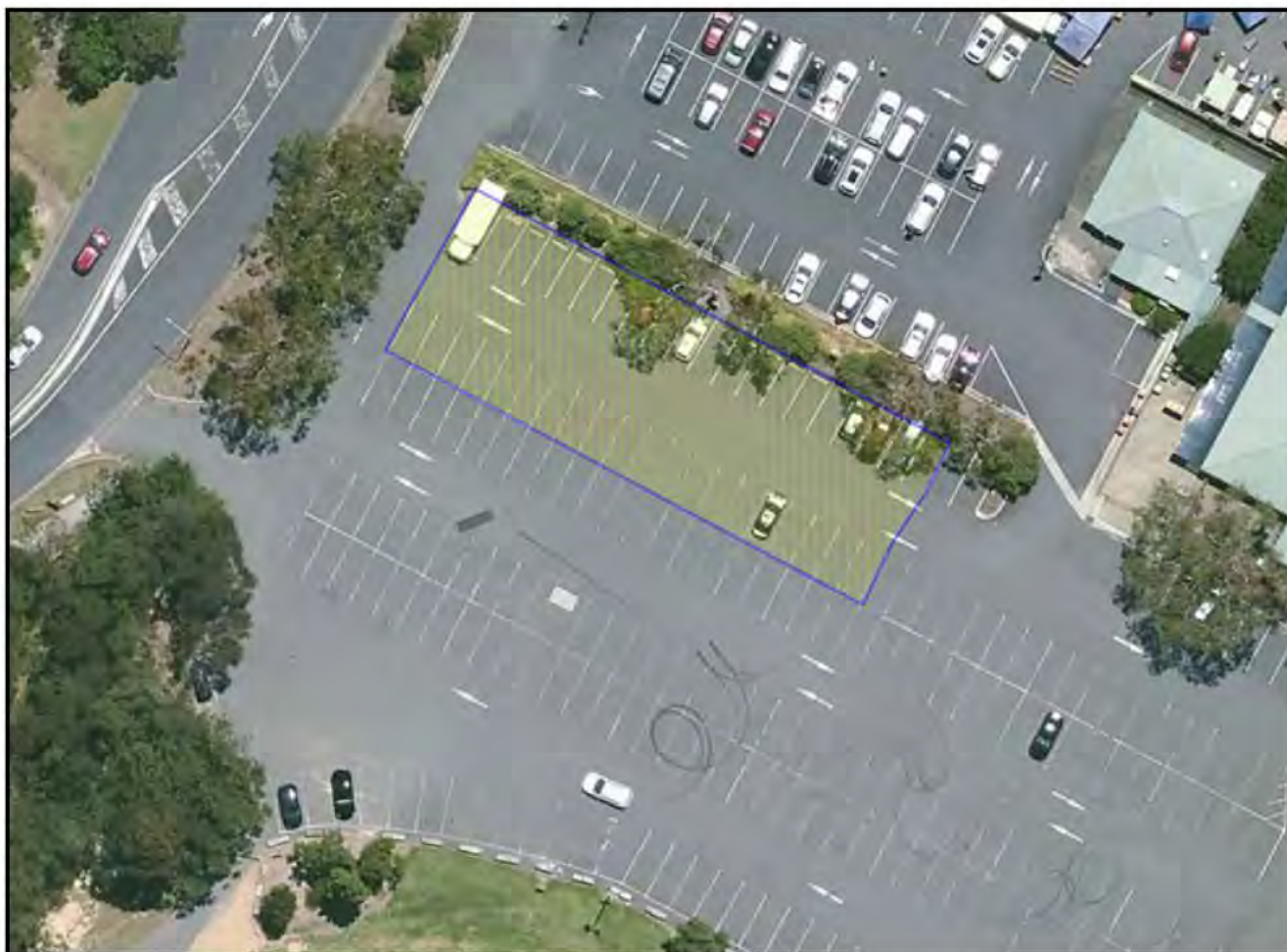


FIGURE 4: ST LUKES INDICATIVE PARKING AREA



3.6 NORTH STRATHFIELD

3.6.1 SITE INFORMATION

The North Strathfield construction site is located adjacent to the east side of the North Strathfield Railway Station fronting Queen Street. The site boundary will extend into Queen Street and beyond the existing parking spaces on the west side of Queen Street.

3.6.2 PARKING DEMAND

There are 15 vehicle parking spaces within the North Strathfield construction site. Workforce and staff numbers around the North Strathfield is not expected to exceed 56 workers.

With the North Strathfield worksite being located next to North Strathfield train station, the project expects a large number of workforce will use public transport to travel to the worksite. This will be encouraged, monitored and adjusted as needed should there be complaints in relation to worker parking on local roads.

Alternative paid parking options are in the area however these are not anticipated to be necessary; this will be further investigated if and when required.

Further information on mitigations and bus services can be found within Section 4.8

FIGURE 5: NORTH STRATHFIELD



3.7 SYDNEY OLYMPIC PARK

3.7.1 SITE INFORMATION

The Sydney Olympic Park construction site is located to the east of Olympic Boulevard between Herb Elliott Avenue and Figtree Drive, surrounded by Sydney Olympic Park town centre and central precinct.

3.7.2 PARKING DEMAND

There are 63 car and 10 motorcycle off street worker parking spaces within the Sydney Olympic Park construction site. Workforce and staff numbers around the Sydney Olympic Park site is not expected to exceed 70 workers. Additional parking is available for workforce and office staff in nearby paid parking facilities within Sydney Olympic park.

Further information on mitigations and bus services can be found within Section 4.8.

FIGURE 6: SYDNEY OLYMPIC PARK



4. PARKING REMOVAL

Some public on-street parking will require temporary removal during the Project to:

- Cater for relocated bus stops and kiss and ride zones
- Improve public safety for the community and workforce
- Better cater for the expected vehicle turning points and movements of trucks and other large vehicles entering and exiting the construction sites
- Reduce traffic congestion behind large vehicles accessing sites; and
- Facilitate localised and temporary construction activities (utilities and construction works as an example)

The temporary removal of existing on-street parking has been discussed through targeted stakeholder consultation in advance with:

- City of Canada Bay council (relevant officers)
- Burwood Council (relevant officers)
- Sydney Olympic Park Authority (relevant officers)
- Transport for NSW (relevant Project officers)
- Local businesses (as identified during local on-street parking and door-knock survey to establish special circumstances e.g loading zones, regular delivery times/days) within 50m area from the identified parking spaces
- Local residents including sensitive receivers (as identified during local on-street parking and door-knock survey to establish any special circumstances e.g. meals on wheels delivery) within a 50m area from the identified parking spaces
- Localised removal of parking to facilitate construction works will be communicated with stakeholders and residents as part of normal works notification and consultation measures

4.1 THE BAYS

No impacts to parking or property access is anticipated.

4.2 FIVE DOCK

4 2 1 EASTERN CONSTRUCTION SITE

About 12 off-street restricted parking spaces that are accessible from Second Avenue have been permanently removed. In addition, up to 9 on-street parking spaces have been temporarily removed near the site access and egress points along Waterview Street, and Second Avenue (unrestricted parking) Parking removal will accommodate construction vehicle movements

TABLE 3: FIVE DOCK EAST INDICATIVE PARKING REMOVAL

Site Reference	Parking removal in the EIS	Parking Spaces Type	Removal Qty Proposed	Removal Period (approx.) for CTP Works	
				From	To
Waterview Street (In front of site, western side)	10	Non marked Untimed	5	19 th April, 2022	June 2025
Second Avenue (In front of site, southern side)		Non marked Untimed	4	19 th April, 2022	June 2025

Site Reference	Parking removal in the EIS	Parking Spaces Type	Removal Qty Proposed	Removal Period (approx.) for CTP Works	
				From	To
Second Avenue Carpark (southern side)	12	2P - 8:30am-6pm Mon-Sat	12	10 th January 2022	Permanent (part of worksite)

Note: the period of parking removal is associated with AFJVs works only and does not consider the following stages of Sydney Metro West. In some locations, parking removal remains at handover from AFJV to Sydney Metro in accordance with the CTP Deed requirements.

4.2.1.1 JUSTIFICATION FOR EXCEEDING THE EIS IDENTIFIED PARKING REMOVAL

There is no exceedance in parking removal from what was identified within the EIS for the Five Dock east site. It's noted that one less parking space will be removed from what was identified within the EIS

4.2.2 WESTERN CONSTRUCTION SITE

12 on street parking spaces have been temporarily removed along the western side of Great North Road between Fred Kelly Place and Second Avenue, adjacent to the Five Dock Station western construction site.

Parking removal of Great North Road has been critical in maintaining public safety and facilitating safe construction work areas to date. Now that bulk excavation has been completed along with other various construction works quieting down, an opportunity has been identified to reinstate three of these parking spaces for public use. The three parking spaces would be reinstated in late March 2024, before being removed again at the end of 2024 to facilitate demobilisation works. At completion of the demobilisation works, these three spaces would then be reinstated for public use again. This is further detailed within Table 4 below.

TABLE 4: FIVE DOCK WEST INDICATIVE PARKING REMOVAL

Site Reference	Parking removal in the EIS	Parking Spaces Type	Removal Qty Proposed	Removal Period (approx.) for CTP Works	
				From	To
Great North Road (In front of site, western side)	12	Untimed	9	21 st March, 2022	Mid 2025
			3*		March 2024
		1/2P 24/7	3	December 2024**	April 2025**

Note: the period of parking removal is associated with AFJVs works only (approx. contract dates included in the table above) and does not consider the following stages of Sydney Metro West. In some locations, parking removal remains at handover from AFJV to Sydney Metro in accordance with the CTP Deed requirements.

* Three parking spaces to be reinstated in late March 2024 and will be time restricted 30-minute parking on their reinstatement.

**Dates provided for the removal and reinstatement of parking are indicative only, dates may need to change due to a number of circumstances including but not limited to weather and construction.

programs. Updates will be provided at TTLG with all relevant community notifications in place prior to each change to the parking arrangements.

4.2.2.2 JUSTIFICATION FOR EXCEEDING THE EIS IDENTIFIED PARKING REMOVAL

There is no exceedance in parking removal from what was identified within the EIS for the Five Dock west site. By reinstating three parking spaces as detailed above, this would be a reduction to the EIS numbers.

4.3 BURWOOD NORTH

About four on-street parking spaces would be temporarily removed on the western side of Loftus Street adjacent to the Burwood North Station northern construction site boundary. Two on-street parking spaces would be temporarily removed on the northern side of Burton Street opposite site exit to allow for safer truck turning. One on-street parking space has been temporarily removed on the southern side of Burton Street as part of the driveway construction.

To allow for the extension of the northbound bus zone on Burwood Road. Two on-street parking spaces have been temporarily removed on the western side of Burwood Road, south of Esher Lane bus zone.

Off-street private parking for properties fronting Burwood Road and Parramatta Road within the Burwood North Station northern construction site would be permanently removed. These parking spaces serve properties that would be demolished during construction and would result in negligible parking impacts.

Parking removal at Burwood North is further detailed below, within Table 5.

TABLE 5: BURWOOD NORTH INDICATIVE PARKING REMOVAL

Site Reference	Parking removal in the EIS	Parking Spaces Type	Removal Qty Proposed	Removal Period (approx) for CTP Works	
				From	To
Loftus Street (In front of site, western side)	4	1/2P - 8:30am-6pm Mon-Fri 830am-1230pm Sat	4	19 th May, 2022	October 2025
Burton Street (Opposite of site, northern side)	0	Non marked Untimed	2	21 st March, 2022	October 2025
Burton Street (In front of site, southern side)	0	Non marked Untimed	1	21 st March, 2022	October 2025
Burwood Road between Esher Lane and 9 Burwood Road (western side)	0	2P - 9:30am-3:30pm Mon-Fri, 8:00am 1:00pm Sat & Public Holidays, No Stopping 6:00am-9:30am Mon-Fri, 3:30pm 7:00pm Mon-Fri	2	7 th March 2022	October 2025

Note: the period of parking removal is associated with AFJVs works only (approx. contract dates included in the table above) and does not consider the following stages of Sydney Metro West. In

some locations, parking removal remains at handover from AFJV to Sydney Metro in accordance with the CTP Deed requirements.

4.3.2.1 JUSTIFICATION FOR EXCEEDING THE EIS IDENTIFIED PARKING REMOVAL

Five additional parking spaces will be temporarily removed from what was identified within the EIS.

- Three additional parking spaces were removed on Burton Street, the removal of these parking spaces was unavoidable due to construction of a driveway and the swept paths of heavy vehicles turning in/out of the driveway. Removal of the parking spaces not only facilitated construction of the driveway but allows for the safe movement and interaction of both work and public vehicles
- While section 4 13 6 of the EIS did identify that relocation of bus stops would be required, the EIS did not account for two parking spaces needing to be removed as a result of extending a bus stop on Burwood Road, south of Parramatta Road, western side. The removal of these two additional parking spaces was unavoidable, without creating unacceptable impacts to public transport.

The additional parking removal noted above is as detailed within the approved Burwood North CTMP

4 4 NORTH STRATHFIELD

To enable construction works, the parking lane on the western side of Queen Street would be temporarily removed between Wellbank Street and Pomeroy Street, resulting in the temporary loss of about 20 on-street parking spaces. These parking spaces are time-restricted to one or two hours.

The Bus stop and Kiss-and-Ride on the western side of Queen Street was relocated to Wellbank Street resulting in temporary loss of 4 on street parking spaces.

On the eastern side of Queen Street, north of Wellbank Street, 1 on-street parking space was removed to accommodate the new pedestrian crossing.

On the eastern side of Queen Street, south of Wellbank Street, 2 on-street parking spaces was removed to accommodate the new pedestrian crossing

Parking removal at North Strathfield is further detailed below, within Table 6.

TABLE 6: NORTH STRATHFIELD INDICATIVE PARKING REMOVAL

Site Reference	Parking removal in the EIS	Parking Spaces Type	Removal Qty Proposed	Removal Period (approx) for CTP Works	
				From	To
Queen Street between Wellbank Street and Pomeroy Street (In front of site, western side)	20 Parking spaces	1P – 9:30am-3:30pm Mon-Fri, 9:30am-12:30pm Sat-Sun, No Parking – 6:30am-9:30am Mon-Fri, 3:30pm-6:30pm Mon-Fri	2	4 th October 2022	September 2025
		2P – 8:30am-6pm Mon-Fri, 8:30am-12:30pm Sat	11		
		Non Marked, Untimed	7		

Site Reference	Parking removal in the EIS	Parking Spaces Type	Removal Qty Proposed	Removal Period (approx.) for CTP Works	
				From	To
Wellbank Street between Queen Street and 158 Wellbank Street (southern side)	0	2P – 8:30am-6pm Mon-Fri, 8:30am 12:30pm Sat	1	3 rd March 2023	September 2025
		Non marked, Untimed	3		
Queen Street between Wellbank Street and 73 Queen Street (eastern side)	0	Non marked Untimed	2	3 rd March 2023	September 2025
Queen Street between Pomeroy Street and 129 Queen Street (eastern side)	0	Non marked Untimed	1	3 rd March 2023	September 2025

Note: the period of parking removal is associated with AFJVs works only (approx contract dates included in the table above) and does not consider the following stages of Sydney Metro West. In some locations, parking removal remains at handover from AFJV to Sydney Metro in accordance with the CTP Deed requirements

4.4.2.1 JUSTIFICATION FOR EXCEEDING THE EIS IDENTIFIED PARKING REMOVAL

Seven additional parking spaces will be temporarily removed from what was identified within the EIS.

- Sections 4.12.5 and 4.12.6 of the EIS identified a need to relocate the kiss and ride zone, and the bus stop from the western side of Queen Street. The EIS did not identify the parking impact that would result from the relocations. Removal of four additional parking spaces on Wellbank Street was unavoidable, without resulting in unacceptable impacts to public transport.
- Section 4.12.7 of the EIS identified the need to close the western footpath on Queen Street, between Wellbank Street and Pomeroy Street. The EIS did not identify the resulting impacts to parking required to cater for the change of pedestrian movements, including two pedestrian crossings, one near Pomeroy Street and one just south of Wellbank Street. The removal of these three additional parking spaces was unavoidable without compromising the safety of pedestrians

4.5 SYDNEY OLYMPIC PARK

No impacts to parking or property access are anticipated.

4.6 PARKING AVAILABILITY

Parking surveys have been conducted during the COVID lockdown period of October 2021. The survey period was not able to avoid the lockdown period from July 2021 to October 2021. Additional parking surveys were also completed at select locations at Five Dock from 7th May 2023 and at Burwood North/Concord from 6th February 2023 with the intention of identifying areas with low public utilisation of on street parking facilities

The parking surveys were conducted in accordance with CoA D91(c) and in line with the 'Patrol Survey' methodology outlined in the Austroads Guide to Traffic Management Part 3, Section H 3.3

This includes an observer patrolling along a predetermined route at fixed intervals, recording the location of parked vehicles to provide information on total parking demand, arrival and departure rates, parking duration and spatial distribution.

CoA D91(c) requires the parking surveys be conducted to cover peak and off-peak periods, school pick up and drop-off periods and weekend periods. The times that have been adopted are detailed in Table 7 below

Parking survey results are included in Appendix 6.1 for reference with results also tabled within section 4.6.4.

TABLE 7 PARKING ASSESSMENT TIMES

Period	Times assessed	
Peak	Weekdays 6am to 10am	Weekdays 3pm to 7pm
Off-peak	Weekdays 10am to 3pm	
School pick-up	Weekday 2:30pm to 3:30pm	
School drop-off	Weekday 8:30am to 9:30am	
Weekend	Saturday 9am to 4pm	Sunday 9am to 4pm

It is noted that during school periods and peak periods some areas are clearways, and therefore the spaces are required to be vacated during these times

4.6.3 PARKING SUPPLY

Tables below provides a summary of the existing parking supply at roads immediately adjacent to the project construction sites. Full details of the surveys including locations and dates of the surveys can be found within Appendix 6.1 with results also tabled within section 4.6.4

TABLE 8 PARKING SUPPLY ADJACENT TO THE BAYS

Site Reference	Parking Space Type	Spaces
Robert Street (northern side)	2P 6am 4pm Mon Fri, Untimed at other times	38
Robert Street (southern side)	Untimed	59
	Total	97

TABLE 9 PARKING SUPPLY ADJACENT TO FIVE DOCK

Site Reference	Parking Space Type	Spaces
Waterview Street	Non marked Untimed	18

Site Reference	Parking Space Type	Spaces
between First Avenue and Second Avenue (eastern side)		
Waterview Street between First Avenue and Second Avenue (western side)	2P – 8:30am-6pm Mon-Fri, Untimed at other times Non marked Untimed	4 16
Waterview Street between Second Avenue and Barnstaple Road (eastern side)	Non marked Untimed	14
Waterview Street between Second Avenue and Barnstaple Road (western side)	2P – 8:30am-6pm Mon-Fri, Untimed at other times Non marked Untimed	10 5
Second Avenue between Great North Road and Waterview Street (northern side)	1/2P 8:30am-6pm Mon-Fri, 8:30am-12:30pm Sat, Untimed at other times	4
Second Avenue between Great North Road and Waterview Street (southern side)	1/2P 8:30am-6pm Mon-Fri, 8:30am-12:30pm Sat, Untimed at other times	4
Second Avenue between Waterview St and Sutton St (northern side)	Non marked Untimed	10
Second Avenue between Waterview St and Sutton St (southern side)	Non marked Untimed	7
Great North Road between East Street and Second Avenue (western side – in front of site)	1/2P 8:30am-6pm Mon-Fri, 8:30am-12:30pm Sat, Untimed at other times	12
	Total	104

TABLE 10 PARKING SUPPLY ADJACENT TO BURWOOD NORTH

Site Reference	Parking Space Type	Spaces
Burton Street Between Broughton St and Lansdowne St (northern side)	Untimed	10
Burton Street Between Lansdowne St and Burwood Rd (northern side)	Untimed	9
Burton Street Between Broughton St and Burwood Rd (southern side)	Untimed	19
Burton Street Between Burwood Rd and Loftus St (northern side)	Untimed	20
Burton Street Between Burwood Rd and Loftus St (southern side)	Untimed	21
Burwood Road Between Burton St and Parramatta Rd (eastern side)	No Parking 6:30am-9:30am Mon-Fri, 3:30pm-6:30pm Mon-Fri, 1/2P – 9:30am-3:30pm Mon-Fri, 8:30am-12:30pm Sat, Untimed at other times	6
Burwood Road Between Parramatta Rd and Milton St (eastern side)	No Stopping 6:30am-9:30am Mon-Fri, 3:30pm-7pm Mon-Fri, 2P 9:30am-3:30pm Mon-Fri, 8am-1pm Sat & Public Holidays, Untimed at other times	5
Burwood Road Between Burton St and Parramatta Rd (western side)	No Stopping 6:30am-9:30am Mon-Fri, 3:30pm-7pm Mon-Fri, 2P – 9:30am-3:30pm Mon-Fri, 8am-1pm Sat & Public Holidays, Untimed at other times	8
Gipps Street Between Burwood Rd and Loftus St (northern side)	Untimed	16

Site Reference	Parking Space Type	Spaces
Gipps Street Between Burwood Rd and Loftus St (southern side)	Untimed	13
Loftus Street Between Parramatta Rd and Burton St (eastern side)	Untimed	5
Loftus Street Between Parramatta Rd and Burton St (western side)	Untimed	8
Loftus Street Between Burton St and Gipps St (eastern side)	Untimed	11
Loftus Street Between Burton St and Gipps St (western side)	Untimed	7
Parramatta Road Between Burwood Rd and Loftus St (northern side)	Clearway 6am-7pm Mon-Fri, 8am-8pm Sat & Sun No Stopping	0
Parramatta Road Between Burwood Rd and Loftus St (southern side)	Clearway 6am-7pm Mon-Fri, 8am-8pm Sat & Sun No Stopping	0
Lansdowne Street Between Gipps St and Burton St (eastern side)	Untimed	23
Lansdowne Street Between Gipps St and Burton St (western side)	Untimed	20
	Total	201

TABLE 11 PARKING SUPPLY ADJACENT TO NORTH STRATHFIELD

Site Reference	Parking Space Type	Spaces
Queen Street between Pomeroy St and Waratah St (eastern side)	Untimed	9
Queen Street between Pomeroy St and Waratah St (western side)	2P 8:30am-6pm Mon Fri, 8:30am 12:30pm Sat, Untimed at other times	4
Queen Street between Waratah St and Wellbank St (eastern side)	1P – 8:30am-6pm Mon-Fri, 8:30am-12:30pm Sat, Untimed at other times	9
Queen Street between Pomeroy St and Wellbank St (western side)	1P 8:30am-6pm Mon Fri, 8:30am 12:30pm Sat, Untimed at other times	5
	No Parking – 6:30am-9:30am Mon-Fri, 3:30pm-6:30pm Mon Fri, 1P – 9:30am-3:30pm Mon-Fri, 9:30am-12:30pm Sat-Sun, Untimed at other times	2
Queen Street between Wellbank St and Shipley Ave (eastern side)	Untimed	13
Queen Street between Wellbank St and Shipley Ave (western side)	No Parking 6:30am 9:30am Mon Fri, 3:30pm 6:30pm Mon-Fri, 1P – 9:30am-3:30pm Mon-Fri, 9:30am-12:30pm Sat-Sun,	1
	Disable parking	1
	2P 8:30am-6pm Mon Fri, 8:30am 12:30pm Sat, Untimed at other times	2

Site Reference	Parking Space Type	Spaces
Wellbank Street between Queen St and 115 Wellbank St (northern side)	2P – 8:30am-6pm Mon-Fri, 8:30am-12:30pm Sat, Untimed at other times	3
Wellbank Street between Queen St and 158 Wellbank St (southern side)	2P 8:30am-6pm Mon Fri, 8:30am 12:30pm Sat, Untimed at other times	1
	Total	50

TABLE 12 PARKING SUPPLY ADJACENT TO SYDNEY OLYMPIC PARK

Site Reference	Parking Space Type	Spaces
Herb Elliott Avenue 4 6 Herb Elliott Ave St (southern side)	Untimed Ticketed Parking	15
Herb Elliott Avenue 10 Herb Elliott Ave St (southern side)	P15min & P2min	12

4.6.4 PARKING SURVEY RESULTS

This section of the CPAS details results of the parking surveys. A full copy of the survey results can also be found within Appendix 6 1

It is important to note that the initial parking surveys were conducted during 2021 where the Greater Sydney region was under COVID lockdown. It is expected that on street parking was higher as residents were forced to stay within their residence

4 6.4 1 THE BAYS

Parking survey results for Robert Street are outlined below.

TABLE 13 THE BAYS PEAK WEEKDAY PARKING DEMAND

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6 10am)	School Drop Off (8:30-9:30am)	Off Peak (10am 3pm)	School Pick Up (2:30-3:30pm)	Weekday (3 7pm)
Robert Street (northern side)	10 th September 2021 to	38	31	28	34	27	27

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30-9:30am)	Off Peak (10am-3pm)	School Pick Up (2:30-3:30pm)	Weekday (3-7pm)
Robert Street (southern side)	16 th September 2021	59	17	17	17	16	17

The parking demands records shows a high weekday parking demand along northern side of Robert Street throughout the day and low demand along the southern side of Robert Street

TABLE 14 THE BAYS PEAK WEEKEND PARKING DEMAND

Site Reference	Date of parking survey	Total Spaces Available	Peak parking demand (During permitted parking times where limited)	
			Saturday (9am-4pm)	Sunday (9am-4pm)
Robert Street (northern side)	10 th September 2021 to 16 th September 2021	38	21	19
Robert Street (southern side)	10 th September 2021 to 16 th September 2021	59	15	14

The results indicate a moderate to low parking demand on Robert Street during the weekend

4.6.4.2 FIVE DOCK

Parking survey results around Five Dock Station are outlined below.

TABLE 15 FIVE DOCK PEAK WEEKDAY PARKING DEMAND

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30-9:30am)	Off Peak (10am-3pm)	School Pick Up (2:30-3:30pm)	Weekday (3-7pm)
Waterview Street between First Avenue and Second Avenue (eastern side)	9 th October 2021 to 15 th October 2021	18	16	15	18	16	16
Waterview Street between First Avenue and Second Avenue (western side)	9 th October 2021 to 15 th October 2021	20	20	20	20	19	20
Waterview Street	9 th October 2021 to	14	14	14	14	13	13

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30-9:30am)	Off Peak (10am-3pm)	School Pick Up (2:30-3:30pm)	Weekday (3-7pm)
between Second Avenue and Barnstaple Road (eastern side)	15 th October 2021						
Waterview Street between Second Avenue and Barnstaple Road (western side)	9 th October 2021 to 15 th October 2021	15	11	11	13	11	11
Second Avenue between Great North Road and Waterview Street (northern side)	9 th October 2021 to 15 th October 2021	4	4	2	3	3	3
Second Avenue between Great North Road and Waterview Street (southern side)	9 th October 2021 to 15 th October 2021	4	4	4	4	4	4
Second Avenue between Waterview St and Sutton St (northern side)	9 th October 2021 to 15 th October 2021	10	10	10	10	10	10

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30-9:30am)	Off Peak (10am-3pm)	School Pick Up (2:30-3:30pm)	Weekday (3-7pm)
Second Avenue between Waterview St and Sutton St (southern side)	9 th October 2021 to 15 th October 2021	7	6	6	7	6	6
Great North Road between East Street and Second Avenue (western side in front of site)	9 th October 2021 to 15 th October 2021	12	12	12	12	12	12
Barnstaple Rd Trevanion St To Bennett Ave (north side)	7 th May 2022 to 13 th May 2022	22	5	5	6	6	8
Barnstaple Rd Bennett Ave To Betts Ave (north side)	7 th May 2022 to 13 th May 2022	12	1	1	1	1	1
Barnstaple Rd Ingham Ave To Park Rd (south side)	7 th May 2022 to 13 th May 2022	62	7	13	14	15	17
Ingham Ave Barnstaple Rd To Noble St (east side)	7 th May 2022 to 13 th May 2022	12	5	5	5	4	4
Ingham Ave Noble St To Howley St (east side)	7 th May 2022 to 13 th May 2022	13	0	0	0	0	0
Ingham Ave Howley St To First Ave (east side)	7 th May 2022 to 13 th May 2022	11	5	6	5	5	5

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30-9:30am)	Off Peak (10am-3pm)	School Pick Up (2:30-3:30pm)	Weekday (3-7pm)
Ingham Ave First Ave To Barnstaple Rd (west side)	7 th May 2022 to 13 th May 2022	47	14	15	15	15	14

The parking demands records shows a high weekday parking demand around Five Dock Station with much lower demand on Barnstaple Road and Ingham Avenue.

TABLE 16 FIVE DOCK PEAK WEEKEND PARKING DEMAND

Site Reference	Date of parking survey	Total Spaces Available	Peak parking demand (During permitted parking times where limited)	
			Saturday (9am-4pm)	Sunday (9am-4pm)
Waterview Street between First Avenue and Second Avenue (eastern side)	9 th October 2021 to 15 th October 2021	18	11	8
Waterview Street between First Avenue and Second Avenue (western side)	9 th October 2021 to 15 th October 2021	20	11	12
Waterview Street between Second Avenue and Barnstaple Road (eastern side)	9 th October 2021 to 15 th October 2021	14	12	11
Waterview Street between Second Avenue and Barnstaple Road (eastern side)	9 th October 2021 to 15 th October 2021	15	9	9
Second Avenue between Great North Road and Waterview Street (northern side)	9 th October 2021 to 15 th October 2021	4	3	4
Second Avenue	9 th October 2021 to 15 th	4	4	4

Site Reference	Date of parking survey	Total Spaces Available	Peak parking demand (During permitted parking times where limited)	
			Saturday (9am-4pm)	Sunday (9am-4pm)
between Great North Road and Waterview Street (southern side)	October 2021			
Second Avenue between Great North Road and Coronation Avenue (northern side)	9 th October 2021 to 15 th October 2021	10	5	3

Site Reference	Date of parking survey	Total Spaces Available	Peak parking demand (During permitted parking times where limited)	
			Saturday (9am-4pm)	Sunday (9am-4pm)
Second Avenue between Great North Road and Coronation Avenue (southern side)	9 th October 2021 to 15 th October 2021	7	6	3
Great North Road between East Street and Second Avenue (western side – in front of site)	9 th October 2021 to 15 th October 2021	12	12	12
Barnstaple Rd Trevanion St To Bennett Ave (north side)	7 th May 2022 to 13 th May 2022	22	7	6
Barnstaple Rd Bennett Ave To Betts Ave (north side)	7 th May 2022 to 13 th May 2022	12	1	1
Barnstaple Rd Ingham Ave To Park Rd (south side)	7 th May 2022 to 13 th May 2022	62	13	14
Ingham Ave Barnstaple Rd To Noble St (east side)	7 th May 2022 to 13 th May 2022	12	6	6
Ingham Ave Noble St To Howley St (east side)	7 th May 2022 to 13 th May 2022	13	0	0
Ingham Ave Howley St To First Ave (east side)	7 th May 2022 to 13 th May 2022	11	5	5
Ingham Ave First Ave To Barnstaple Rd (west side)	7 th May 2022 to 13 th May 2022	47	20	21

The weekend parking demands shows a moderate parking demand around Waterview Street and Second Avenue at Five Dock Station with high demand on Great North Road and low demand Barnstaple Road and Ingham Avenue.

4.6.4.3 BURWOOD NORTH

Parking survey results around Burwood North Station are outlined below.

TABLE 17 BURWOOD NORTH PEAK WEEKDAY PARKING DEMAND

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30-9:30am)	Off Peak (10am-3pm)	School Pick Up (2:30-3:30pm)	Weekday (3-7pm)
Burton Street Between Broughton St and Lansdowne St (northern side)	10 th September 2021 to 16 th September 2021	10	10	10	10	10	10
Burton Street Between Lansdowne St and Burwood Rd (northern side)	10 th September 2021 to 16 th September 2021	9	7	7	8	7	7
Burton Street Between Broughton St and Burwood Rd (southern side)	10 th September 2021 to 16 th September 2021	19	17	17	16	16	17
Burton Street Between Burwood Rd and Loftus St (northern side)	10 th September 2021 to 16 th September 2021	20	19	19	20	19	18
Burton Street Between Burwood Rd and Loftus St (southern side)	10 th September 2021 to 16 th September 2021	21	17	17	18	18	17
Burwood Road Between Burton St and Parramatta Rd (eastern side)	10 th September 2021 to 16 th September 2021	6	0	0	3	3	1
Burwood Road	10 th September	5	4	4	4	4	4

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30-9:30am)	Off Peak (10am-3pm)	School Pick Up (2:30-3:30pm)	Weekday (3-7pm)
Between Parramatta Rd and Milton St (eastern side)	2021 to 16 th September 2021						
Burwood Road Between Burton St and Parramatta Rd (western side)	10 th September 2021 to 16 th September 2021	8	3	2	7	6	8
Gipps Street Between Burwood Rd and Loftus St (northern side)	10 th September 2021 to 16 th September 2021	16	11	11	12	12	9
Gipps Street Between Burwood Rd and Loftus St (southern side)	10 th September 2021 to 16 th September 2021	13	13	11	14	12	14
Loftus Street Between Parramatta Rd and Burton St (eastern side)	10 th September 2021 to 16 th September 2021	5	5	5	5	5	5
Loftus Street Between Parramatta Rd and Burton St (western side)	10 th September 2021 to 16 th September 2021	8	8	8	8	8	8
Loftus Street Between Burton St and Gipps St (eastern side)	10 th September 2021 to 16 th September 2021	11	10	10	11	10	9
Loftus Street Between Burton St and Gipps St (western side)	10 th September 2021 to 16 th September 2021	7	7	7	7	7	7

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30-9:30am)	Off Peak (10am-3pm)	School Pick Up (2:30-3:30pm)	Weekday (3-7pm)
Parramatta Road Between Burwood Rd and Loftus St (northern side)	10 th September 2021 to 16 th September 2021	0	0	0	0	0	0
Parramatta Road Between Burwood Rd and Loftus St (southern side)	10 th September 2021 to 16 th September 2021	0	0	0	0	0	0
Lansdowne Street Between Gipps St and Burton St (eastern side)	10 th September 2021 to 16 th September 2021	23	7	7	7	7	7
Lansdowne Street Between Gipps St and Burton St (western side)	10 th September 2021 to 16 th September 2021	20	20	7	7	5	7
Broughton St Gipps St To Crane St (western side)	6 th February 2023 to 12 th February 2023	33	10	11	13	15	15
Broughton St Gipps St to David St (eastern side)	6 th February 2023 to 12 th February 2023	9	4	4	4	3	4
Broughton St David St to Stanley St (eastern side)	6 th February 2023 to 12 th February 2023	12	6	6	7	7	6
Broughton St Stanley St to Crane St (eastern side)	6 th February 2023 to 12 th February 2023	15	8	7	7	9	9

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30-9:30am)	Off Peak (10am-3pm)	School Pick Up (2:30-3:30pm)	Weekday (3-7pm)
Addison Ave Patterson St To Flavelle St (eastern side)	6 th February 2023 to 12 th February 2023	84	12	12	11	11	16
Addison Ave Patterson St To Flavelle St (western side)	6 th February 2023 to 12 th February 2023	35	4	3	4	4	4
Gipps St Flavelle St to Broughton St (north side)	6 th February 2023 to 12 th February 2023	9	2	2	2	2	1
Gipps St Flavelle St to Broughton St (south side)	6 th February 2023 to 12 th February 2023	10	0	0	0	0	1
Gipps St Burwood Rd to Cintra Hockey Complex (north side)	6 th February 2023 to 12 th February 2023	16	4	5	6	6	6
Gipps St Burwood Rd to Loftus St (south side)	6 th February 2023 to 12 th February 2023	13	9	9	8	7	10
Gipps St Loftus St to Cintra Hockey Complex (south side)	6 th February 2023 to 12 th February 2023	10	1	2	2	2	1

The parking demands shows a high weekday parking demand around Burwood North Station during weekday period

TABLE 18 BURWOOD NORTH PEAK WEEKEND PARKING DEMAND

Site Reference	Date of parking survey	Total Spaces Available	Peak parking demand (During permitted parking times where limited)	
			Saturday (9am-4pm)	Sunday (9am-4pm)
Burton Street Between Broughton St and Lansdowne St (northern side)	10 th September 2021 to 16 th September 2021	10	9	9
Burton Street Between Lansdowne St and Burwood Rd (northern side)	10 th September 2021 to 16 th September 2021	9	6	5
Burton Street Between Broughton St and Burwood Rd (southern side)	10 th September 2021 to 16 th September 2021	19	15	14
Burton Street Between Burwood Rd and Loftus St (northern side)	10 th September 2021 to 16 th September 2021	20	15	18
Burton Street Between Burwood Rd and Loftus St (southern side)	10 th September 2021 to 16 th September 2021	21	15	12
Burwood Road Between Burton St and Parramatta Rd (eastern side)	10 th September 2021 to 16 th September 2021	6	1	2
Burwood Road Between Parramatta Rd and Milton St (eastern side)	10 th September 2021 to 16 th September 2021	5	3	5
Burwood Road Between Burton St and Parramatta Rd (western side)	10 th September 2021 to 16 th September 2021	8	5	7
Gipps Street Between Burwood Rd and Loftus St (northern side)	10 th September 2021 to 16 th September 2021	16	7	7

Site Reference	Date of parking survey	Total Spaces Available	Peak parking demand (During permitted parking times where limited)	
			Saturday (9am-4pm)	Sunday (9am-4pm)
Gipps Street Between Burwood Rd and Loftus St (southern side)	10 th September 2021 to 16 th September 2021	13	12	9
Loftus Street Between Parramatta Rd and Burton St (eastern side)	10 th September 2021 to 16 th September 2021	5	5	3
Loftus Street Between Parramatta Rd and Burton St (western side)	10 th September 2021 to 16 th September 2021	8	7	4
Loftus Street Between Burton St and Gipps St (eastern side)	10 th September 2021 to 16 th September 2021	11	5	5
Loftus Street Between Burton St and Gipps St (western side)	10 th September 2021 to 16 th September 2021	7	6	3
Parramatta Road Between Burwood Rd and Loftus St (northern side)	10 th September 2021 to 16 th September 2021	0	0	0
Parramatta Road Between Burwood Rd and Loftus St (southern side)	10 th September 2021 to 16 th September 2021	0	0	0
Lansdowne Street Between Gipps St and Burton St (eastern side)	10 th September 2021 to 16 th September 2021	23	7	5
Lansdowne Street Between Gipps St and Burton St (western side)	10 th September 2021 to 16 th September 2021	20	6	7

The weekend parking demands shows a high parking demand around Burwood North Station with moderate demand on Loftus Street and low parking demand on Lansdowne Street.

4.6.4.4 NORTH STRATHFIELD

Parking survey results around North Strathfield Station are outlined below.

TABLE 19 NORTH STRATHFIELD PEAK WEEKDAY PARKING DEMAND

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30-9:30am)	Off Peak (10am-3pm)	School Pick Up (2:30-3:30pm)	Weekday (3-7pm)
Queen Street between Pomeroy St and Waratah St (eastern side)	13 th October 2021 to 19 th October 2021	9	5	5	6	6	6
Queen Street between Pomeroy St and Waratah St (western side)	13 th October 2021 to 19 th October 2021	4	4	4	4	4	4
Queen Street between Waratah St and Wellbank St (eastern side)	13 th October 2021 to 19 th October 2021	9	9	9	9	9	9
Queen Street between Pomeroy St and Wellbank St (western side)	13 th October 2021 to 19 th October 2021	7	7	7	7	7	7
Queen Street between Wellbank St and Shipley Ave (eastern side)	13 th October 2021 to 19 th October 2021	13	12	12	12	11	10
Queen Street between Wellbank St and Shipley Ave (western side)	13 th October 2021 to 19 th October 2021	4	4	2	4	3	4
Wellbank Street	13 th October 2021 to	3	3	3	3	3	3

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30-9:30am)	Off Peak (10am-3pm)	School Pick Up (2:30-3:30pm)	Weekday (3-7pm)
between Queen St and 115 Wellbank St (northern side)	19 th October 2021						
Wellbank Street between Queen St and 158 Wellbank St (southern side)	13 th October 2021 to 19 th October 2021	1	1	1	1	1	1

The parking demands shows a high weekday parking demand around North Strathfield Station.

TABLE 20 NORTH STRATHFIELD PEAK WEEKEND PARKING DEMAND

Site Reference	Date of parking survey	Total Spaces Available	Peak parking demand (During permitted parking times where limited)	
			Saturday (9am-4pm)	Sunday (9am-4pm)
Queen Street between Pomeroy St and Waratah St (eastern side)	13 th October 2021 to 19 th October 2021	9	4	4
Queen Street between Pomeroy St and Waratah St (western side)	13 th October 2021 to 19 th October 2021	4	4	4
Queen Street between Waratah St and Wellbank St (eastern side)	13 th October 2021 to 19 th October 2021	9	9	9
Queen Street between Pomeroy St and Wellbank St (western side)	13 th October 2021 to 19 th October 2021	7	7	7
Queen Street between Wellbank St and Shipley Ave (eastern side)	13 th October 2021 to 19 th October 2021	13	10	10

Site Reference	Date of parking survey	Total Spaces Available	Peak parking demand (During permitted parking times where limited)	
			Saturday (9am-4pm)	Sunday (9am-4pm)
Queen Street between Wellbank St and Shipley Ave (western side)	13 th October 2021 to 19 th October 2021	4	3	3
Wellbank Street between Queen St and 115 Wellbank St (northern side)	13 th October 2021 to 19 th October 2021	3	3	3
Wellbank Street between Queen St and 158 Wellbank St (southern side)	13 th October 2021 to 19 th October 2021	1	1	1

4.6.4.5 SYDNEY OLYMPIC PARK

Parking survey results around Sydney Olympic Park Station are outlined below

TABLE 21 SYDNEY OLYMPIC PARK PEAK WEEKDAY PARKING DEMAND

Site Reference	Date of parking survey	Total Spaces Available	Average peak parking demand (During permitted parking times where limited)				
			Weekday (6-10am)	School Drop Off (8:30- 9:30am)	Off Peak (10am- 3pm)	School Pick Up (2:30- 3:30pm)	Weekday (3-7pm)
Herb Elliott Avenue 4-6 Herb Elliott Ave St (southern side)	18 th October 2021 to 24 th October 2021	15	11	11	15	12	14
Herb Elliott Avenue 10 Herb Elliott Ave St (southern side)	18 th October 2021 to 24 th October 2021	12	7	7	7	6	7

The parking demands shows a high weekday parking demand around the eastern side of Herb Elliott Avenue, while the short term parking along the western side of Herb Elliott Avenue are moderately occupied

TABLE 22 SYDNEY OLYMPIC PARK PEAK WEEKEND PARKING DEMAND

Site Reference	Date of parking survey	Total Spaces Available	Peak parking demand (During permitted parking times where limited)	
			Saturday (9am-4pm)	Sunday (9am-4pm)
Herb Elliott Avenue 4-6 Herb Elliott Ave St (southern side)	18 th October 2021 to 24 th October 2021	15	11	1
Herb Elliott Avenue 10 Herb Elliott Ave St (southern side)	18 th October 2021 to 24 th October 2021	12	7	7

Parking demand remain high during Saturday while demand drops off on Sunday.

4 6 5 SPECIAL EVENTS

At The Bays, there is no parking removal in the surrounding area. Therefore, there will be no changes to parking availability and impact during special events

At Five Dock, The Ferragosto is an all-day event typically held on a Sunday in August which will be outside of construction working hours of the Project. There are no additional parking spaces removed outside of approved EIS spaces, therefore AFJV expect minimal impact to parking

At Burwood North, there are no special events around Burwood Road and Parramatta Road near the work site. AFJV is working closely with City of Canada Bay Council through regular meetings and will consider impact to events held in Concord Oval.

There are no special events held near the North Strathfield construction site

There is no parking removal in Sydney Olympic Park and no impact to parking availability. Sydney Olympic Park Authority will provide AFJV with their special event management plan which will direct AFJV to use specific roads during special events. These roads will also comply with approved heavy vehicle routes.

4 7 CONCLUSION OF THE PARKING DEMAND SURVEY

4.7.1 THE BAYS

Around The Bays, there is some parking available along Robert Street but will not be sufficient to cover surplus parking requirement from the AFJV project. Extra parking facilities have been sourced to accommodate peak construction activities

4 7 2 FIVE DOCK

Parking demand around Five Dock is high during weekdays and weekends. The removal parking spaces along Waterview Street, Second Avenue, and Great North Road (as per EIS) will be absorbed into the surrounding streets. There are insufficient spare parking capacity to accommodate workforce parking adjacent to the construction sites. Alternative roads with underutilised parking have been identified to accommodate worker parking during peak construction activities where workers parking on public roads cannot be entirely eliminated.

4.7.3 BURWOOD NORTH

Parking demand around Burwood North is high during weekdays and weekends with minimal parking spaces around the construction site to accommodate workforce parking adjacent to the project sites. Alternative roads with underutilised parking have been identified to accommodate

worker parking during peak construction activities where workers parking on public roads cannot be entirely eliminated.

4.7.4 NORTH STRATHFIELD

Parking demand around North Strathfield is high during weekdays and weekends with minimal parking spaces adjacent to the construction site. This is likely due to the parking demand from North Strathfield train Station. Due to the construction site being located next to the North Strathfield train station, it's expected that a large number of workers will utilise public transport. The use of public transport will be promoted, with effectiveness monitored.

4.7.5 SYDNEY OLYMPIC PARK

Parking demand around Sydney Olympic Park is high during weekdays and Saturdays with minimal parking spaces around the construction site. There are sufficient parking space within the Sydney Olympic Park construction site to accommodate up to 55 workers. Alternative paid parking options within Sydney Olympic Park have been identified to accommodate any overflow requirements.

4.8 MITIGATION MEASURES

4.8.1 MINIMISE PARKING ON PUBLIC ROADS

Through detailed construction planning and consultation with City of Canada Bay Council, Sydney Olympic Park Authority and NSW Port Authority; a variety of measures have been implemented to minimise workers parking on public roads in accordance with CoA D90(a) of the Planning Approval). These measures include:

- Underground excavation at Five Dock was managed from the East site which has meant that AFJV have consolidated the workforce to the greatest extent possible (ie not having multiple work fronts).
- From the end of February 2024 construction works from the Five Dock western site have been limited, a turn table installed at the southern access gate and creation of up to 4 off road parking spaces for work vehicles at the northern access gate.
- Shift patterns for mined tunnelling at Burwood North and Five Dock have been limited to five (5) days per week instead of 7 days, minimising impact to public roads during weekend periods.
- Interfaces have been optimised from excavation and lining works such that when TBMs are traversing through Five Dock there are no concurrent activities. This minimises the volume of workforce that may need to be at the site at one time.
- Five Dock is not being used to manage day to day TBM logistics and materials in order to minimise the volume of workforce, deliveries and heavy vehicles at this location.
- The methods of excavation at Five Dock have been chosen to minimise workforce requirements (eg surface miner has 2 operators as opposed to 5-6 excavators with individual operators).
- AFJV has implemented an automated direct to truck kibble system for spoil loading at Five Dock, Burwood North and North Strathfield sites. This minimises the spoil load out activity to one person (removes upper and lower dogman).
- AFJV has maximised the size of concrete pours to limit the number of days specialist concrete activities are happening on site (which in turn reduces the number of days subcontractors are in attendance at the sites);
- AFJV has implemented an automated guidance systems on all excavation equipment which reduces the need for surveyor attendance to site; and

- AFJV encourages car pooling (and informal coordination of shift workers who live in the same area so car pooling can be more easily facilitated).
- AFJV will implement and encourage the use of public transport through an internal communications strategy for the life of the Project. This will be achieved by:
 - The employee induction process, advise potential employees of the limited nature of on site parking, travel allowances provisions, the need to behave as respectful guests in the community and generally encourage the use of public transport, where possible. This will enable workers to make an informed decision about their transport options when they join the Project
 - Encourage the use of apps such as “Opal Travel” for transport services and timetables
- Advising the workforce through toolbox talks and prestart meetings, on where parking is and is not permitted.
- Leasing land from the Port Authority within the existing Port area at the Bays, approximately 500m from site. This facilitates an area where workforce can park, eliminating the need for worker parking on public roads in the area, further noting that approximately 70% of project workforce operate from this site.
- Using the off street paid parking facilities at Sydney Olympic Park (P1, P2, P3 & P4), with a worker reimbursement scheme implemented by AFJV to ensure use of the existing off-street parking facilities.
- Extending use of the off street parking facilities at Sydney Olympic Park, and encouraging workers to carpool to the North Strathfield site from the Sydney Olympic Park paid parking facilities.
- Where possible, provide bicycle and motor-bike parking facilities including end of trip facilities on AFJV sites.
- Where possible, offer working from home opportunities to reduce onsite staff.
- A shuttle bus service to transport workers to be implemented as further detailed below in Section 4.8.2. This includes continual promotion of Olympic Park shuttle bus service to target personnel who are not based on site but need to visit site as part of their roles (eg environmental / safety / quality inspections) and continued use of the Bays shuttle bus service to cater for periods of TBM traverse through Burwood North and Five Dock.
- Use of 40 off-street worker parking spaces at St Lukes Carpark has been secured in consultation with CoCBC. Regular surveillance and consultation with CoCBC will be undertaken to ensure no impact through the use of St Lukes Carpark by AFJV personnel. If deficiencies are identified, consultation would explore alternative options such as an increase to the agreed parking allocation.
- As part of the procurement process, options to reduce workers parking on local roads will be further explored with subcontractors performing the concrete lining works at the Five Dock construction sites, expected to commence in August 2024. This may include the subcontractors providing a shuttle service from their compound or car pooling.
- As part of removing onsite worker parking during demobilisation of the Burwood North and Five Dock sites, reassess worker and staff numbers to minimise impact of worker parking. This will be completed with the aim of relocating workers to the Sydney Olympic Park office where practical

The overall strategy of ‘minimising parking on public roads’ always remains the primary goal. It is however noted that complete elimination of worker parking on public roads is not achievable at all sites. For this reason, additional surveys of on street parking facilities have taken place at Burwood North and Five Dock sites with the aim of identifying existing on-street parking facilities that are

underutilised by the public. Where a complete elimination of worker parking on public roads is not achievable, workers are instructed to park in these areas to mitigate the potential impact to community.

Streets identified for worker parking is as shown below within Figure 7 and Figure 8. Parking survey results are also tabled within section 4.6.4 and a full copy provided within Appendix 6.1.

FIGURE 7: FIVE DOCK ADDITIONAL WORKER PARKING

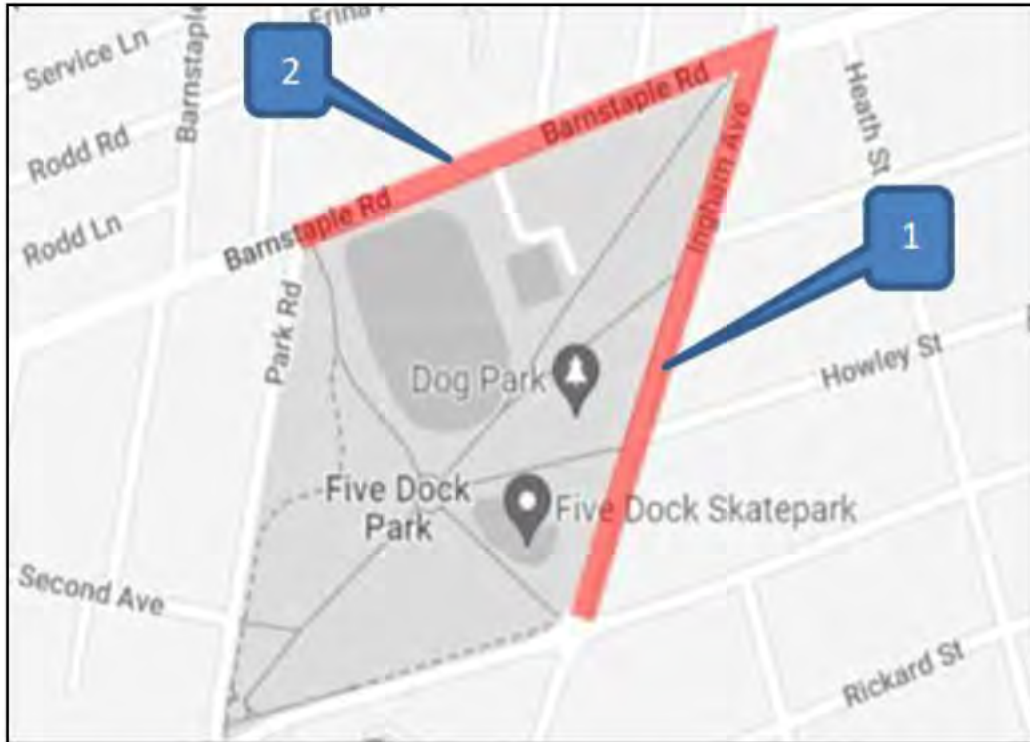


FIGURE 8: BURWOOD NORTH ADDITIONAL WORKER PARKING



4.8.2 AFJV SHUTTLE BUSES

This section of the CPAS provides details for the shuttle bus services proposed for implementation as part of the revision 06 CPAS. There will be two primary services intended for use during TBM operations, one based at Sydney Olympic Park and one based at The Bays, both of these services are further detailed below.

4.8.2.1 AFJV BUS SERVICE - THE BAYS

A bus service to transport workers associated with TBM traverse operations at Five Dock, Burwood North and North Strathfield as required will be implemented. The Bays site has been selected due to the available off-road parking area. The service will start in the mornings picking up workers from the parking area at The Bays, workers will then be transported to their respective sites. Throughout the day typically between 8:30am and 3:30pm, bus services will be utilised to transport workers between The Bays and the different sites on an 'on-demand/as needed' basis. It is expected that the need to transport workers through the day would be minimal. At the end of shift, workers will be collected from each of their respective sites and transported back to The Bays parking area.

4.8.2.2 AFJV BUS SERVICE - SYDNEY OLYMPIC PARK

A shuttle bus will be used to provide an alternative transport service for workers at North Strathfield, Burwood North and Five Dock. Sydney Olympic Park site has been selected due to its proximity to the Sydney Olympic station transport hub and ample off-road paid public parking options. The service will start in the mornings picking up workers from the Sydney Olympic Park Site, workers will then be transported to their respective sites. Throughout the day typically between 8:30am and 3:30pm, bus services will be utilised to transport workers between Sydney Olympic Park and the different sites on an 'on-demand/as needed' basis. It is expected that the need to transport workers through the day would be minimal to moderate. At the end of shift, a shuttle bus will provide an alternative transport service back to the Sydney Olympic Park site.

4.8.2.3 AFJV BUS SERVICE - ROUTES AND STOPS

Bus routes and stopping locations have been determined by considering multiple factors including, worker demand, locations to be serviced, safe worker pedestrian routes and where buses can stop safely & without the need to remove further public parking.

Buses with a maximum expected length of 6m and weight under 4.5t will be used. With a weight under 4.5t, buses will not be restricted to the use of existing approved haul routes, however existing haul routes will be used where practical. Where possible, bus stop locations will be inside of the sites to avoid impacting public areas, noting this is not possible at the Five Dock site. Bus service times and frequency will be adjusted as required to match any changes to shift start and end times, as well as any increase or decrease to worker demand.

A full copy of the VMPs for the AFJV bus services can be found within Appendix 6.5

4.8.3 ADDITIONAL MITIGATION MEASURES UNDER CONSIDERATION

- Review staff who are based on site at Burwood and Five Dock to identify opportunities to recentralise some of the positions back to the Olympic Park office, following completion of peak construction activities. Staff would then utilise the Sydney Olympic Park shuttle bus service to attend their respective sites on an as needed basis.
- Following re-launch of the TBMs at Five Dock station construction site, review site-based roles to identify whether any of the roles can be serviced from the TBM team. This would enable a reduction of site-based workforce at Five Dock and Burwood.
- Review number of work vehicles allocated to the Five Dock and Burwood North teams, and if possible, reduce the quantity allocated.

- Exploring options for paid parking at both Five Dock and Burwood North for staff

4.8.4 CONTINGENCY MEASURES

4.8.4.4 THE BAYS

The Bays site has passed peak construction activities, with the current secured off-street parking facilities proving sufficient. For this reason, issues with available parking are not anticipated.

4.8.4.5 FIVE DOCK

The Five Dock site has passed peak construction activities, for this reason issues with available parking and the current arrangements is not anticipated.

4.8.4.6 BURWOOD NORTH

Burwood North has passed peak construction activities, for this reason issues with available parking and current arrangements is not anticipated.

4.8.4.7 NORTH STRATHFIELD

The North Strathfield site is nearing completion of peak construction activities. The site will continue being monitored and if on street parking becomes an issue due to higher demand for parking spaces than anticipated, additional measures will be employed which may include increasing parking availability for workers, such as:

- The paid off-street parking facilities at Railway Street, Homebush
- The paid public parking facilities at Sydney Olympic Park to increase carpooling between the sites.

4.8.4.8 SYDNEY OLYMPIC PARK

Sydney Olympic Park has passed peak construction activities. Additionally, a large range of off-street paid parking facilities exist, for this reason parking issues are not expected to occur at this site.

4.8.5 MINIMISE IDLING AND QUEUING ON STATE AND REGIONAL ROADS

AFJV will advise construction drivers during induction (physical or virtual) that idling and queuing on state and regional roads needs to be minimised and avoided.

AFJV will also utilise LinkedSite (LS) which is an in-house company developed program designed to track and analyse construction vehicle movement in and around projects. During site congestion, LS will be used to advise drivers of the delay, and to either wait at the spoil site or be redirected to other sites. This will reduce the likelihood of idling and queuing on state and regional roads.

Additionally, a heavy vehicle marshalling area has been identified for use, this is further detailed below within Section 4.8.6.

4.8.6 MARSHALLING OF CONSTRUCTION VEHICLES

To minimise the requirement for project marshalling of trucks the spoil team works closely with haulage companies to space out the returning trucks from tip sites to the worksites. This will minimise impact to sensitive land users and reduce the likelihood of idling and queuing on state and regional roads. Where locations of limited receivers and sensitive land users can be identified which fronts onto a state or regional road, these locations will be communicated as options for marshalling trucks to avoid queues onto public roads at sites where on-site storage is limited.

To date, one location for heavy vehicle marshalling has been identified as detailed below. Further marshalling areas are being investigated, if additional areas are identified for use, the relevant plans will be updated to reflect:

4 8 6 1 THE BAYS

In consultation with Port Authority, an area approximately 350m from The Bays site was secured. The area was capable of marshalling up to 8 trucks and primarily serviced the Bays construction site. As of mid-June 2024, Port Authority require this section of land to be handed back to make way for works associated with the Western Harbour Tunnel project (WHT). A revised agreement has been reached with the NSW Port Authority and Sydney Metro to secure a different section of land on Glebe Island. This new section of land will not be capable of supporting truck marshalling. For this reason, an existing area previously used to service the site grout plant will be utilised to provide a marshalling area for up to two heavy vehicles, each 19m in length.

It is noted that at this stage of the project, the need for truck marshalling has significantly reduced. The project is also delivering concrete tunnel segments to the Burwood North site, in turn reducing the quantity of segment deliveries to The Bays, and continues to implement the various existing controls detailed within this CPAS. This includes, staggered truck bookings and use of Linked Site. These two marshalling spaces being provided are intended as a contingency only.

Heavy vehicle swept paths for this marshalling area can be found within Appendix 6 4

FIGURE 11: THE BAYS TRUCK MARSHALLING AREA



4.8.7 OTHER MITIGATION MEASURES

AFJV has considered staged removal of parking around the station worksite. At Burwood North Station, impact to parking on Burton Street and Loftus Street near the driveways coincide with the use and construction of the access driveways. Dates of the staged parking removal is as detailed within section 4.

AFJV has also staged parking removal at North Strathfield station to reduce the impact to surrounding businesses, residents, and impact to public transport. Parking on Queen Street western

side was removed followed by parking removal on Wellbank Street and other select locations on Queen Street to accommodate installation of pedestrian crossing facilities in accordance with the approved CTMP. Dates of the staged parking removal is as detailed within section 4. Alternative parking arrangements for local residents and businesses are not practicable as there are insufficient parking areas around the worksite. AFJV has provided alternative parking areas for its workforce and there is sufficient parking space to minimise the impact to local residents

Parking restrictions adjacent to work sites have been limited as any restrictions will take away residential or business parking. AFJV has tried to limit parking removal to accommodate heavy vehicle movements, vehicle access and to improve road safety

There are currently no residential parking schemes around The Bays Station, Burwood North Station, North Strathfield Station, and SOPA stations. A Permit Parking Scheme was implemented by City of Canada Bay Council around the Five Dock Town Centre in November 2023. This will further incentivise workers to park at the designated parking locations. Further information on City of Canada Bay council's parking permit strategy can be found on the council's website <https://collaborate.canadabay.nsw.gov.au/fivedockpps>

4.9 MONITORING

4.9.1 INSPECTION

As part of the ongoing monitoring processes on the project, parking assessment and monitoring will play a vital role of the surveillance team's responsibility. Monitoring will include attending sites on random days and times to identify workers parking on public roads. This will provide indication of the effectiveness of alternative arrangements, and mechanisms of encouraging workers to park at the parking facilities and catch public transport.

One inspection will be conducted per month as a minimum, from the commencement of tunnelling. The inspections will assess workers parking outside of designated parking areas within the surrounding streets. The results will be reported to the project Traffic Manager for reporting and disseminated to the relevant site Project Managers for intervention for those not following the site transport and parking protocol.

AFJV utilises LinkedSite which is an in-house company developed program designed to track and analyse construction vehicle movement in and around projects. LS has the following capabilities:

- Induction heavy vehicle checks, plant pre-start inspections and fatigue management
- GPS tracking of construction vehicles around and between sites, and
- Data collection and analysis.

LS assists driver compliance (including compliance to haulage routes) through digital induction form as they begin work. This includes a plant pre-start form and fatigue management questionnaire. Operator hours are automatically logged and communication capability can be used to instruct operators to take their allocated breaks.

LS is capable of analysing real-time weather and traffic data, allowing AFJV to manage its fleets more efficiently by predicting arrival times and communicate directly with workers. For example, an influx of trucks arriving on site can be directed to take an alternative route or travel to a designated marshalling area.

The GPS tracking capability also enables AFJV to determine the speed and location of the fleet, automatically generating heat maps for vehicle movements. Fleet movement is better managed around site by determining pinch-points and adjusted accordingly. Furthermore, each load can be individually traced back to the location received and laid.

When moving between the construction sites, LS analyses construction vehicle movements to help better understand driver behaviour. Alerts can be set up to notify key personnel when a vehicle is speeding or using undesignated roads.

In response to Concept and Stage 1 Conditions of Approval D83, records of monitoring be made available electronically to the Planning Secretary and the EPA upon request for a period of no less than one (1) year following the completion of construction.

Where additional local roads are required this will be used following approval by DPIE in accordance with CoA D86. LS will be used to ensure spoil haulage vehicles adhere to the nominated haulage route identified in the CTMP. A sample of heavy vehicle access plan (vehicle movement plan) can be found in Appendix 6.2

4.9.2 REPORTING

Monthly updates on compliance will be reported internally and determine any additional requirements, or controls to be implemented

A 6 monthly report of monitoring results will be sent to the Planning Secretary and Relevant Council(s) in accordance with D91(k). The report will be provided as a standalone report.

4.10 CONTINGENCY

If monitoring, surveys, consultation or complaints prompt intervention by the Project to improve or otherwise modify services, one or more of the options discussed in the sections below may be enacted to ensure impacts to public are reduced and public relations and reputation is protected

4.10.1 ADDITIONAL OVERFLOW PARKING

Parking utilisation will be monitored at each of the parking areas. Where supply is dwindling additional parking will be investigated and provided where practical solutions can be identified.

Investigations will potentially include lease options with adjacent businesses to sites, investigating layout improvements within sites where opportunities arise, or expanding existing parking facilities at other sites and implementing bus services to provide suitable and efficient transport solutions to and from the additional parking

4.10.2 RE-EDUCATION AND CORRECTION

Where workers are impacting the amenity of residents, not complying with the Code of Conduct, or repeatedly behaving or parking inappropriately they may be required to re-attend the project induction which will include detail on the alternative parking options and incentives for parking at the parking facilities. Stronger sanctions, up to and including dismissal, may be implemented for repeat offenders at the discretion of the Project Manager

4.10.3 SHIFT STRUCTURES

The workforce shift times will contemplate, where possible the availability and running times of the nearest public transport options. Typically, the management teams will endeavour to structure their workers into team's that encourage carpooling. This is achieved by grouping crews based on where they reside. Similar shift patterns with a workforce that live near each other, will naturally assist in the increase of carpooling

4.11 ACCESS CONTROL AND SAFETY

4.11.1 PEDESTRIAN INTERFACE WITH DRIVEWAYS

Each of the sites will be assessed as to the demand for pedestrian movements past the access and egress points, especially those which carry heavy vehicles, spoil movements and delivery trucks. Potential mitigation measures include a pedestrian gate to temporarily stop pedestrians as heavy (and some light) vehicles cross their paths

It is acknowledged that the frequency and duration of truck movements for the life of the project may increase the relative risk in those locations. For sites where greater volumes of pedestrians and

increased risk profiles are apparent, additional engineered controls based on risk assessments will be implemented to better control interactions between vehicle and pedestrian movements around and past the access and egress points, to ensure safety is maintained for the duration of the Project. Additional information is provided in the site specific Construction Traffic Management Plans (CTMP).

Pedestrian and shared user paths would not be blocked or disrupted at any time unless alternate access is provided. AFJV will follow the process outlined in the overarching CTMP and will seek approval from TfNSW and/or local council prior to any disruption to pedestrian footpath or shared user paths.

4.11.2 MANAGING PEDESTRIANS

AFJV will implement necessary controls to direct pedestrians, to and from bus stops, rail stations, car parks, businesses, homes and any directly affected schools during construction. AFJV recognises the importance of giving consideration to all road users. AFJV has identified pedestrian needs by considering:

- Impact of construction works on existing footpaths
- Number of pedestrians
- Type of pedestrian activity: office, retail, residential, school or recreational
- Origin and destination points of the pedestrians and their desired travel path
- Existing needs of vulnerable pedestrians, such as young children, the elderly, vision impaired, disabled people and people with prams
- Proximity of pedestrian generation developments, such as schools, bus stops and train stations
- Requirements of the CTMP

Consideration has been given to diversion of pedestrians in the site-specific CTMPs, with designated paths and pedestrian crossing facilities where the existing travel routes are not available. Safety barriers will be installed to segregate works from pedestrian paths and/or pedestrians from traffic flows, where required. All barriers will be maintained and appropriately secured while in use.

AFJV will advise TCG and the relevant road authority (CJP, local council, SOPA and/or TfNSW), prior to adjusting any existing pedestrian crossing facility or the implementation of any new temporary facility. Approval from relevant road authority will be required prior to adjusting any existing pedestrian facility.

AFJV will maintain current formal and informal pedestrian connectivity and functionality provided within and directly adjacent the Project. Pedestrian facilities will be designed to meet relevant guidelines including the Project area by preserving and/or upgrading existing connections, or providing upgraded alternative connections.

AFJV will manage the pedestrian desire lines with temporary footpaths that comply with the requirements of Austroads Guide to Road Design Part 6A: Pedestrians and Cycle Paths, TCaWS and AS 1742.3. Prior to work commencing on State and local roads, where pedestrian access may be affected, alternate pedestrian access routes will be signed and delineated in accordance with safety requirements.

Alternate routes will aim to minimise inconvenience to pedestrians with the primary goal of maintaining clear space between pedestrians and active work areas. This will be addressed in site specific CTMPs prior to the construction activities commencing.

The following measures will be implemented when providing alternate pedestrian routes to minimise impacts on mobility impaired pedestrians:

- Clearly defined temporary footpath arrangements by using appropriate signage
- Sufficient space for wheelchair access

- Smooth, even surface on all temporary footpaths and crossings
- Regular inspections to maintain footpaths free of trip hazards
- Minimisation of grades for wheelchair use.

Where access impacts to business or properties are unavoidable, alternate arrangement will be developed in consultation with affected business and implemented prior to the impact occurring. Whenever access is impacted, signage and directions for businesses will be provided before and during the impacts

For safety reasons pedestrian concertina gate will be provided at each access and exit driveway to enhance the safety of pedestrians AFJV will also consider installation of footpath decals or other measures to increase awareness of the presence of heavy vehicles along high pedestrian areas

4.11.3 MANAGING CYCLIST

Access for cyclists will be maintained during the construction works. Where required, a temporary alternative route detour will be provided. The specific controls implemented at each site location are dependent on traffic volume and cyclists at each affected location. The site-specific CTMP will assess impacts on cyclist and recommended cyclist detours where required

AFJV will redirect cyclists and maintain the existing cycling facilities where possible during the construction works. Haulage routes that traverse existing on-road bicycle routes only occur around the Sydney Olympic Park site as follows where relevant signs and line marking are provided:

- Australia Avenue (525m)
- Sarah Durack Avenue (435m)

It is however noted that the bike map provided in the Canada Bay and Sydney Olympic Park websites indicates additional roads around the site as designated on-road bicycle routes or bike safari circuits but relevant signs and line marking are not provided on these roads.

Cyclist access to the existing on road cycle routes will be maintained where possible. Provision of a "Watch for Cyclist" warning sign will be provided at these locations, as required.

Where existing cyclist facilities (e.g. bicycle parking) will be temporarily unavailable to facilitate construction activities, suitable replacement facilities will be provided for this duration

4.11.4 ACCESS TO RESIDENTS AND BUSINESS

AFJV will ensure the assessment of pedestrian movements past site occur prior to construction commencing and any mitigation measures are implemented. This assessment can be done as part of traffic guidance scheme development and implementation.

Access restrictions to businesses or properties from the works will be avoided where possible. If any business or property access is impact (including by pedestrians, bicycle or vehicle) AFJV will provide alternative access or other mitigation measures where possible. Where access impacts to business or properties are unavoidable, alternate arrangement will be developed in consultation with affected business and implemented prior to the impact occurring. Whenever access is impacted, signage and directions for businesses will be provided before and during the impacts

4.11.5 EMERGENCY ACCESS

Emergency vehicle access would be provided to all properties at all times.

Emergency services will be informed of the defined routes and consulted and advised of any changes in the defined routes. Priority and a safe environment will be provided to emergency vehicles to enable efficient and safe travel through construction areas. The CTMP includes measures to keep emergency services informed of the progress of construction works

4.11.6 CONSTRUCTION SITE ACCESS

To provide safe access and exit to the work sites AFJV is responsible to:

- Install turning truck signs to warn motorists of trucks turning into and out of site accesses
- All trucks are to enter and exit sites in a forward direction, where feasible and reasonable
- Design access and exit driveways that are visible to approaching traffic and signposted accordingly
- Design intersections and access points in accordance with Austroads Guide to Road Design Part 4A Unsignalised and Signalised Intersections, and if / where required relevant standards, guides or manuals
- Manage staff at site access driveways with suitable measures
- Install security fences and gates at locations which maintain clear sight lines
- Traffic Guidance Schemes will be prepared, where required, for temporary changes to the traffic environment associated with compound establishment and use
- Pedestrian Management Plans will be prepared, where required, for temporary changes to pedestrian access resulting from compound establishment and use
- Vehicle Movement Plans will be prepared, where required, for access associated with establishment and use of construction compounds and access routes
- Access for emergency vehicles and to firefighting equipment will be maintained
- Consider installation of footpath decals or other measures to increase awareness of the presence of heavy vehicles along high pedestrian areas
- Consider additional signage around Sydney Olympic Park to raise awareness between heavy vehicles and cyclists.
- Where possible site access and egress should be via state and regional roads (e.g. Parramatta Road)

The AUSTROADS Guide to Traffic Engineering provides guidance on the design of intersections and access points. No matter the type of intersection configuration implemented, temporary traffic controls may be required from time to time to facilitate short term major haulage and the movement of over-dimension vehicles

4.11.7 TRAFFIC IMPACT OF CONSTRUCTION ACTIVITIES

The site-specific CTMPs aim to provide a safe environment for road users, pedestrians, cyclists and workers. Any impact on road users and asset operation and maintenance will be kept to a minimum. Planning and scheduling works are being undertaken to minimise impacts on road users during the commuter peak periods.

Enabling works are to establish the station construction sites and precast facility site. The works involve hoarding, demolition of structures and buildings, construction of access and exit driveways and new traffic facilities, such as installation of new TCS, new pedestrian crossing facilities, one way conversion on local roads and on street parking removal

Site-specific CTMPs will be prepared to assess the following traffic and transport impacts associated with the proposed construction activities:

- Network capacity analysis (where relevant)
- Public transport routes
- Pedestrian and cyclist accessibility
- Local access

- Emergency vehicle access
- Waste collection vehicle access
- Special events

TGSs will be developed to support the works including consultation with TfNSW, TMC, SM, Councils, SOPA and Port Authority. TGSs show the proposed road worksite arrangements to protect the safety of road users as well as workers at site, in accordance with AS1742 3, TfNSW G10 Specification and TCaWS Manual. Consideration will be given to the use of signage, including Variable Message Signage (VMS) to inform motorists, pedestrians and cyclists of changes, delays and diversions where necessary and practical.

Construction site traffic immediately around construction sites would be managed to minimise vehicle movements through school zones during pick up and drop off times.

Where existing parking is removed to facilitate construction activities, consultation will occur with the relevant local council to investigate opportunities to provide alternative parking facilities, where possible.

TfNSW will be provided with any details of traffic incident that occur within proximity of the worksite within two days of the incident.

4.12 CONSULTATION

As part of the approval of CPAS Revision 6, DPHI required the CPAS to be provided to relevant stakeholders (Council and SOPA) for feedback. Any relevant feedback received as an outcome of this consultation will be captured in a future revision of the CPAS and also in a Consultation Report (to be provided to DPHI and included on the Project Website following submission).

Site specific consultation on construction parking is as detailed below.

4.12.8 THE BAYS

During the utilities works required for installation of the TBM water supply to The Bays Construction site, several parking spaces were temporarily impacted for a period of approximately 3 weeks to facilitate these works. Consultation with businesses started in December 2022 with a door knock all businesses impacted by the changes to parking during the January school holidays. Notification outlining all the approved traffic and parking changes around The Bays was distributed to community within 500m of The Bays site on mid December. One business stated that the changes would have a large impact on their operations, by removing the parking adjacent to their main entrance. Following a review of the safety reasons for the removal, AFJV agreed to adjust the plans and returned 3 of the 8 spaces removed.

AFJV were also required to put in place a pedestrian diversion on Crescent Street for the utility work taking place. Consultation via a door knock took place with residents in the street to outline the safety reasons for proposing the temporary pedestrian diversion. No issues or concerns were raised. This was also included in the monthly notification process.

No other parking changes have resulted from works associated with The Bays Construction Site and, as such, no further consultation has been required.

4.12.9 FIVE DOCK

Consultation with residents started in late November 2021 with an online meeting with residents/owners of 110 Great North Road. Notification outlining all the proposed traffic and parking changes around Five Dock was distributed to the community within 500m of the Five Dock site on 15 December. Nearby businesses/residents of Second Avenue and Waterview Street were door-knocked throughout December 2021 and January 2022. Concerns raised included workers parking in local streets of Five Dock and inconvenience associated with the proposal to make a section of Second Avenue to one-way. No other specific

issues were raised in response to the extensive letterbox drop or during the targeted meetings with local residents and businesses however apart from a general concern about lack of parking around the Five Dock CBD.

In early February 2022, AFJV also presented to the Five Dock Public School P&C via an online zoom meeting. Attendees noted they were aware of the upcoming traffic and parking changes around Five Dock from the December 2021 notification and did not raise concerns about it but they did have concern on how worker parking may impact the overall area based on previous WestConnex experience

The February 2022 Five Dock newsletter was distributed on 21 February and included an update for the community that feedback received in December and January was being reviewed and implementation of the traffic changes in Five Dock is pending approval of the CTMP with specific dates to be provided once confirmed. Confirmation of previously flagged parking removal along Great North Road was included in the notification distributed on 4 March 2022. Confirmation of the traffic changes and parking removal along Second Avenue and Waterview Street was included in the notification distributed on 31 March 2022. AFJV also hosted a community drop-in session at Fred Kelly Place, Five Dock ahead of the traffic and parking changes occurring on 19 April 2022.

A series of meeting and presentations have been held with City of Canada Bay Council to discuss arrangements for worker parking around the Five Dock site. This included a review of the parking surveys as detailed in Appendix 6.1. A summary of this consultation has been provided to Department of Planning for information.

4.12.10 BURWOOD NORTH

AFJV undertook extensive consultation with businesses in Burwood during March 2022 for the bus stop relocations from Parramatta Road in front of the construction sites to Burwood Road, resulting in modifications to the planned bus stop street furniture along Burwood Road to assist outdoor dining for local cafes. A notification outlining the traffic and parking changes was distributed on 31 March 2022 to the community as well as signage and traffic control to assist direct passengers to the new stop locations. Consultation was also undertaken with local residents on Loftus and Burton streets for parking changes around the construction sites to enable site access. Concerns raised included loss of parking in front of food establishments and worker parking in local streets. A series of meeting and presentations have been held with City of Canada Bay Council to discuss arrangements for worker parking around the Burwood North site. This included a review of the parking surveys as detailed in Appendix 6.1. A summary of this consultation has been provided to Department of Planning for information.

4.12.11 NORTH STRATHFIELD

Consultation for the removal of parking along the western side of Queen Street began in March 2022 with the issuing of a Newsletter to the community within 500m of the North Strathfield metro station site. This was followed by door-knocking local businesses, Sydney Trains, schools and residents throughout August, followed by a second Newsletter in August 2022 and a Notification distributed on 26 August 2022. Core flute posters were placed around the site in advance of the work to ensure Sydney Trains passengers were also aware of the changes. Concerns raised included the loss of parking for the nearby businesses and workers parking in the remaining spots. AFJV clearly outlined the reasons for the parking removal as the area was within the construction footprint of the new metro station site. AFJV also door-knocked residents during January and February 2023 for the bus stop relocation from Queen Street to Wellbank Street as well as the pedestrian crossing installation and the parking changes required to enable this. A notification was distributed to the surrounding community on 30 January 2023.

Community consultation for any future changes to parking or access across the project sites will be carried out with the affected community in advance of the proposed changes being implemented.

4 12 12 SYDNEY OLYMPIC PARK

Consultation with Sydney Olympic Park Authority has been undertaken throughout the CTP Works by AFJV through monthly interface meetings and weekly communications meeting (including special event review and management) There has been no parking changes as a result of the CTP Works at Sydney Olympic Park with parking being provided at the Project office at 7 Figtree Drive and the construction site at 5 Figtree Drive.

5. CONCLUSION

The CPAS survey has identified the following,

At Five Dock, there is a low volume of spare parking capacity to absorb spaces removed as part of the project. The number of parking space removed on Waterview Street, Second Avenue and Great North Road are the same as outlined in the EIS, and no additional parking space is taken

At Burwood North, there is spare parking capacity on Burton Street, Lansdowne Street, and Gipps Street to absorb any loss in parking surrounding the site.

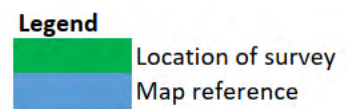
At North Strathfield, there is a low volume of spare parking capacity to absorb spaces removed as part of the project. Removal of parking spaces on the western side of Queen Street is in accordance with the approved EIS. Additional parking space taken on Wellbank Street is to accommodate the bus zone, and kiss and ride relocation

- At Sydney Olympic Park, workforce parking is fully contained within the worksite and other established off street paid parking areas. There are currently no plans to reduce on street parking

A number of mitigation measures were identified to reduce impact of workforce parking to the local community, as detailed within Section 4.8.

6. APPENDICES

6.1 PARKING SURVEY SUMMARY



Friday 10th September 2021				Date		10/09/2021																												
Street	Side	Area	Restriction	Map reference	Supply	Time																												
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Robert Street	S	Between Robert St and Start of On-Road Cycleway	Unrestricted	1	5	2	3	2	2	2	2	1	2	3	4	5	4	2	2	4	6	3	4	4	4	4	3	1	2	2	2	2		
Robert Street	S	Between Return and Earn Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted 90 deg Rear-to-Kerb	2	59	12	12	13	13	14	12	12	14	14	14	13	13	14	15	14	14	14	14	14	14	13	13	13	13	12	12	12		
Robert Street	N	Between Robert St and 30 m east of Port Access Rd Exit Driveway*	2P 6AM-4PM MON-FRI 90deg Rear-to-Kerb & Unrestricted 90 deg Rear-to-Kerb	3	38	22	22	23	25	23	26	28	28	31	32	33	34	24	27	22	21	26	23	25	27	24	18	16	14	13	13	12		
Robert Street	N	Between Port Access Rd Exit Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted	4	3	1	1	1	1	1	2	2	2	2	2	2	3	1	1	2	2	2	2	2	1	1	1	1	1	1	1	1		
Total occupied						37	38	39	41	40	42	43	46	50	52	53	54	41	45	42	43	44	46	42	36	31	30	29	28	27	29	27		
Supply						105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	
Saturday 11th September 2021				Date		11/09/2021																												
Street	Side	Area	Restriction	Map reference	Supply	Time																												
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Robert Street	S	Between Robert St and Start of On-Road Cycleway	Unrestricted	1	5	2	2	2	2	2	2	1	2	3	4	5	4	2	2	4	6	3	4	4	5	3	1	2	2	2	2			
Robert Street	S	Between Return and Earn Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted 90 deg Rear-to-Kerb	2	59	10	11	12	12	13	13	13	13	13	14	14	14	14	14	14	15	15	15	15	15	15	15	13	13	12	12	12		
Robert Street	N	Between Robert St and 30 m east of Port Access Rd Exit Driveway*	2P 6AM-4PM MON-FRI 90deg Rear-to-Kerb & Unrestricted 90 deg Rear-to-Kerb	3	38	10	11	12	12	12	13	18	17	21	18	20	20	20	18	18	19	20	19	17	17	17	15	15	15	13	12	10		
Robert Street	N	Between Port Access Rd Exit Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted	4	3	1	1	1	1	1	1	1	1	1	2	2	2	2	2	2	2	2	2	1	1	1	1	1	1	1	1			
Total occupied						23	25	27	27	28	29	33	33	38	38	41	40	38	36	38	41	39	41	39	37	38	36	32	31	31	28	27	25	26
Supply						105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	
Sunday 12th September 2021				Date		12/09/2021																												
Street	Side	Area	Restriction	Map reference	Supply	Time																												
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Robert Street	S	Between Robert St and Start of On-Road Cycleway	Unrestricted	1	5	2	2	2	2	2	2	2	2	3	2	2	2	2	3	3	2	2	2	2	2	2	3	2	3	2	2			
Robert Street	S	Between Return and Earn Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted 90 deg Rear-to-Kerb	2	59	12	12	12	12	12	12	13	13	13	13	13	13	13	13	13	14	14	14	13	13	13	13	13	13	13	13	13		
Robert Street	N	Between Robert St and 30 m east of Port Access Rd Exit Driveway*	2P 6AM-4PM MON-FRI 90deg Rear-to-Kerb & Unrestricted 90 deg Rear-to-Kerb	3	38	9	9	9	9	9	10	11	10	16	17	17	19	19	19	17	18	19	18	16	9	9	10	10	9	9	9	9		
Robert Street	N	Between Port Access Rd Exit Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted	4	3	1	1	1	1	1	1	1	1	1	2	1	1	1	1	1	1	2	2	1	1	1	1	1	1	1	1			
Total occupied						24	24	24	24	24	25	27	26	33	34	33	35	35	35	36	34	36	37	35	32	25	25	26	27	25	26	25	25	
Supply						105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	
Monday 13th September 2021				Date		13/09/2021																												
Street	Side	Area	Restriction	Map reference	Supply	Time																												
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Robert Street	S	Between Robert St and Start of On-Road Cycleway	Unrestricted	1	5	2	2	5	5	6	6	5	7	5	4	6	4	5	5	6	4	4	3	0	0	5	5	4	3	3	2	2		
Robert Street	S	Between Return and Earn Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted 90 deg Rear-to-Kerb	2	59	14	14	14	14	14	14	14	14	14	14	14	14	14	14	14	14	14	14	14	14	14	14	14	12	12	12	13	12	
Robert Street	N	Between Robert St and 30 m east of Port Access Rd Exit Driveway*	2P 6AM-4PM MON-FRI 90deg Rear-to-Kerb & Unrestricted 90 deg Rear-to-Kerb	3	38	13	14	20	20	18	20	20	22	22	22	21	21	18	17	20	23	16	18	19	19	19	18	13	16	15	11	12	12	
Robert Street	N	Between Port Access Rd Exit Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted	4	3	1	1	1	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	1	1	1	1	1			
Total occupied						30	31	40	41	40	42	41	45	43	42	43	41	39	38	41	43	36	37	35	35	40	40	37	31	32	30	26	28	27
Supply						105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105
Tuesday 14th September 2021				Date		14/09/2021																												
Street	Side	Area	Restriction	Map reference	Supply	Time																												
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Robert Street	S	Between Robert St and Start of On-Road Cycleway	Unrestricted	1	5	2	3	4	4	3	5	3	4	4	3	3	2	2	4	4	4	2	2	4	4	3	3	3	3	2	1	1	1	
Robert Street	S	Between Return and Earn Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted 90 deg Rear-to-Kerb	2	59	14	14	15	16	15	15	15	15	15	12	15	15	15	15	15	15	15	15	15	15	15	15	14	14	14	15	12		
Robert Street	N	Between Robert St and 30 m east of Port Access Rd Exit Driveway*	2P 6AM-4PM MON-FRI 90deg Rear-to-Kerb & Unrestricted 90 deg Rear-to-Kerb	3	38	15	13	17	18	24	23	22	22	22	23	15	22	23	23	23	21	22	23	23	19	19	19	15	13	11	11	12	11	
Robert Street	N	Between Port Access Rd Exit Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted	4	3	2	2	2	2	2	2	2	2	2	2	2	2	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1		
Total occupied						33	32	38	40	44	45	42	43	43	40	35	41	41	43	43	39	40	43	43	39	38	38	34	31	28	27	29	25	
Supply						105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105
Wednesday 15th September 2021				Date		15/09/2021																												
Street	Side	Area	Restriction	Map reference	Supply	Time																												
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Robert Street	S	Between Robert St and Start of On-Road Cycleway	Unrestricted	1	5	3	4	4	4	5	4	4	5	4	5	4	3	4	6	4	5	5	4	5	3	6	3	4	2	2	3	3		
Robert Street	S	Between Return and Earn Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted 90 deg Rear-to-Kerb	2	59	15	15	16	16	17	17	17	17	16	16	16	16	16	16	16	16	16	16	16	16	16	17	11	11	11	9	9	9	

Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM					
Robert Street	N	Between Robert St and 30 m east of Port Access Rd Exit Driveway*	2P 6AM-4PM MON-FRI 90deg Rear-to-Kerb & Unrestricted 90 deg Rear-to-Kerb	3	38	16	15	19	19	21	23	24	24	23	21	23	21	23	22	22	22	26	22	21	21	22	21	20	16	13	10	10	10	11					
Robert Street	N	Between Port Access Rd Exit Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted	4	3	2	2	3	3	3	3	3	3	3	3	3	3	3	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2					
Total occupied						36	36	42	42	46	47	48	49	46	45	46	43	46	46	44	46	49	44	44	42	46	42	43	31	28	25	23	24	25					
Supply						105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105					
Thursday 16th September 2021												Date 16/09/2021																											
												Map reference																											
												Supply																											
Robert Street	S	Between Robert St and Start of On-Road Cycleway	Unrestricted	1	5	4	5	5	6	5	5	5	5	5	5	7	5	7	5	7	6	4	7	4	4	5	4	3	3	2	1	1	1	1					
Robert Street	S	Between Return and Earn Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted 90 deg Rear-to-Kerb	2	59	10	10	11	12	12	12	12	12	12	12	12	12	12	12	12	16	13	13	13	13	13	13	12	10	10	10	10	9	9					
Robert Street	N	Between Robert St and 30 m east of Port Access Rd Exit Driveway*	2P 6AM-4PM MON-FRI 90deg Rear-to-Kerb & Unrestricted 90 deg Rear-to-Kerb	3	38	13	13	14	16	21	19	17	19	22	22	21	29	23	23	23	22	22	21	20	23	25	22	20	16	14	13	13	13	12					
Robert Street	N	Between Port Access Rd Exit Driveway and 50 m east of Port Access Rd Exit Driveway	Unrestricted	4	3	0	0	2	2	2	1	1	1	1	1	1	2	2	2	2	2	2	2	3	2	2	3	2	2	1	1	1	1	1					
Total occupied						27	28	32	36	40	37	35	37	40	40	41	48	44	42	48	43	41	43	40	42	45	41	35	31	27	25	25	24	23					
Supply						105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105	105				



Legend
Location of survey
Map reference

Saturday 9th October 2021				Date		9/10/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																													
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Great N Rd	W	Between East St Ped Crossing and St Alban's	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	1	15	5	16	14	16	21	14	23	27	28	23	31	28	24	27	29	23	29	23	22	15	19	16	19	18	23	15	16	16	14	
Second Ave	S	Between Great N Rd and Waterview St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	2	4	1	1	1	5	3	5	4	6	5	4	5	6	7	4	4	3	3	3	4	2	2	2	3	2	2	4	2	2	2	
Second Ave	N	Between Great N Rd and Waterview St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	4	1	2	2	3	2	1	1	1	3	1	2	2	1	2	2	1	2	2	1	1	2	2	2	2	2	2	2	2	2	
Second Ave	S	Between Waterview St and Sutton St	Unrestricted	4	7	4	4	4	4	4	4	4	5	5	4	6	4	4	4	4	5	5	4	4	4	4	4	4	3	3	3	3	3		
Second Ave	N	Between Waterview St and Sutton St	Unrestricted	5	10	1	1	2	2	2	2	3	3	3	3	4	3	3	5	5	3	3	4	5	4	3	2	2	2	2	2	2	2	2	
Waterview St	W	Between First Ave and Second Ave	2P 830AM-6PM MON-FRI	6	4	0	0	0	0	0	0	1	3	3	2	3	4	4	3	3	3	3	3	3	3	3	3	3	3	3	2	2	2	2	
Waterview St	W	Between First Ave and Second Ave	Unrestricted	7	16	3	3	3	3	3	3	3	3	5	7	6	4	5	6	3	4	4	4	4	6	4	5	5	5	5	5	5	3	3	
Waterview St	E	Between First Ave and Second Ave	Unrestricted	8	18	6	5	4	6	7	7	7	7	6	9	9	9	11	10	9	8	6	9	5	7	6	4	4	5	5	5	3	2	2	
Waterview St	W	Between Second Ave and Barnstaple Rd	Unrestricted	9	5	4	4	4	4	5	4	4	4	3	4	4	3	3	4	4	3	3	4	4	3	2	4	4	3	3	3	3	2	2	
Waterview St	W	Between Second Ave and Barnstaple Rd	2P 830AM-6PM MON-FRI	10	10	3	4	3	3	3	3	4	3	4	3	3	3	4	3	3	4	3	4	5	5	5	4	4	3	3	3	4	4	3	
Waterview St	E	Between Second Ave and Barnstaple Rd	Unrestricted	11	14	10	10	10	9	9	9	12	10	10	10	11	11	9	10	9	9	9	10	12	12	11	12	13	12	13	13	13	13	13	10
Total occupied						38	50	47	55	59	52	66	72	75	70	84	77	74	79	75	65	71	71	69	62	60	58	62	59	64	59	55	51	45	
Supply						107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107

Sunday 10th October 2021				Date		10/10/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																													
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Great N Rd	W	Between East St Ped Crossing and St Alban's	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	1	15	2	4	7	15	18	24	19	17	19	16	23	18	19	14	17	20	17	17	12	10	13	11	19	14	17	17	14	11	10	
Second Ave	S	Between Great N Rd and Waterview St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	2	4	1	1	1	1	1	2	2	2	2	4	4	2	3	3	3	2	2	2	1	1	1	2	2	2	2	1	1	1		
Second Ave	N	Between Great N Rd and Waterview St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	4	1	1	1	2	2	2	2	2	2	3	2	2	2	2	4	3	4	3	2	2	1	1	1	2	1	0	0	0		
Second Ave	S	Between Waterview St and Sutton St	Unrestricted	4	7	3	3	3	3	3	3	3	3	3	3	2	3	3	3	3	3	3	3	2	2	2	2	2	2	2	2	2	2		
Second Ave	N	Between Waterview St and Sutton St	Unrestricted	5	10	1	1	1	1	1	1	1	1	2	2	2	3	1	2	3	2	1	1	2	2	3	2	1	1	1	1	1	1		
Waterview St	W	Between First Ave and Second Ave	2P 830AM-6PM MON-FRI	6	4	1	1	1	1	0	0	0	0	2	3	3	3	2	2	1	2	2	1	1	1	1	2	1	0	0	1	1	0		
Waterview St	W	Between First Ave and Second Ave	Unrestricted	7	16	5	6	6	6	7	7	7	9	8	8	7	7	6	8	5	6	7	8	7	6	6	6	8	7	6	6	5	5	5	
Waterview St	E	Between First Ave and Second Ave	Unrestricted	8	18	4	3	3	3	3	4	4	4	4	5	8	7	5	5	6	5	6	5	5	5	6	6	5	5	5	4	4	2	2	
Waterview St	W	Between Second Ave and Barnstaple Rd	Unrestricted	9	5	4	4	3	3	4	4	4	3	4	4	4	3	3	3	3	3	4	4	3	3	4	3	4	4	5	4	3	3		
Waterview St	W	Between Second Ave and Barnstaple Rd	2P 830AM-6PM MON-FRI	10	10	3	3	3	3	4	4	4	4	3	3	3	3	3	4	4	4	5	5	5	5	5	5	5	5	5	5	5	5	5	
Waterview St	E	Between Second Ave and Barnstaple Rd	Unrestricted	11	14	10	11	11	11	10	10	10	11	10	9	10	10	11	10	9	9	10	9	9	9	10	10	9	9	9	9	9	9	9	
Total occupied						35	38	40	49	53	61	56	56	59	57	69	63	57	55	56	61	58	59	52	46	52	49	57	51	53	52	46	40	38	
Supply						107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107

Monday 11th October 2021				Date		11/10/2021																												
Street	Side	Area	Restriction	Map reference	Supply	Time																												
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Great N Rd	W	Between East St Ped Crossing and St Alban's	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	1	15	2	11	15	16	17	15	13	15	25	14	16	19	11	16	20	19	21	17	21	15	17	19	19	22	21	21	15	16	11
Second Ave	S	Between Great N Rd and Waterview St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	2	4	1	1	3	1	3	4	4	5	5	5	6	6	7	6	4	4	5	4	4	5	5	4	4	2	2	2	2	1	1
Second Ave	N	Between Great N Rd and Waterview St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	4	1	2	2	1	4	2	1	1	3	2	2	2	3	2	2	2	2	2	2	2	2	2	2	2	1	1	1	1	1
Second Ave	S	Between Waterview St and Sutton St	Unrestricted	4	7	2	2	3	3	4	5	5	5	5	5	5	6	7	7	7	5	5	6	5	4	3	4	4	3	3	3	3	3	
Second Ave	N	Between Waterview St and Sutton St	Unrestricted	5	10	1	1	1	2	5	9	9	9	9	9	9	9	9	9	9	9	9	8	8	8	8	8	6	3	2	1	2	2	2

Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Waterview St	W	Between First Ave and Second Ave	2P 830AM-6PM MON-FRI	6	4	0	0	0	1	2	4	5	6	7	7	6	6	6	5	5	6	6	6	4	4	6	4	3	5	3	5	4	2	1
Waterview St	W	Between First Ave and Second Ave	Unrestricted	7	16	6	6	6	5	8	11	12	12	11	11	13	13	12	12	13	14	14	12	12	12	12	11	10	7	6	6	4	3	3
Waterview St	E	Between First Ave and Second Ave	Unrestricted	8	18	3	4	5	9	9	11	12	10	13	14	14	13	15	13	18	13	14	16	13	13	12	11	9	7	7	8	8	7	3
Waterview St	W	Between Second Ave and Barnstaple Rd	Unrestricted	9	5	4	4	4	4	4	4	5	5	5	5	5	5	6	5	5	4	5	5	5	4	4	3	4	3	3	4	4	4	3
Waterview St	W	Between Second Ave and Barnstaple Rd	2P 830AM-6PM MON-FRI	10	10	4	5	5	4	5	5	5	5	5	5	6	7	6	5	4	4	5	6	5	5	5	5	6	5	4	4	5	5	5
Waterview St	E	Between Second Ave and Barnstaple Rd	Unrestricted	11	14	9	10	12	9	9	10	11	14	12	12	11	12	12	12	11	9	10	12	12	10	10	11	12	12	11	9	10	9	9
Total occupied					33	46	56	55	70	80	82	87	100	89	93	98	93	93	98	89	96	94	91	82	84	82	79	71	64	64	58	53	42	
Supply					107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107	107

Tuesday 12th October 2021					Date	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021	12/10/2021
Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Great N Rd	W	Between East St Ped Crossing and St Alban's	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	1	15	11	16	13	12	15	14	16	16	19	17	20	15	22	19	15	18	18	17	15	19	14	20	18	21	20	18	15	16	13	
Second Ave	S	Between Great N Rd and Waterview St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	2	4	2	4	3	2	3	4	4	5	3	4	3	3	3	5	7	7	4	3	4	5	4	3	2	3	4	4	3	2	1	
Second Ave	N	Between Great N Rd and Waterview St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	4	1	1	1	4	2	2	2	1	2	2	2	3	2	2	3	3	3	2	1	1	1	1	1	2	2	2	2	1	1	
Second Ave	S	Between Waterview St and Sutton St	Unrestricted	4	7	3	4	4	4	5	6	6	6	6	6	5	5	5	5	5	5	6	5	5	6	5	5	4	4	4	4	4	4	4	
Second Ave	N	Between Waterview St and Sutton St	Unrestricted	5	10	1	2	2	3	6	7	8	9	9	9	9	9	9	10	10	9	9	11	9	9	9	9	9	8	3	1	1	1	1	
Waterview St	W	Between First Ave and Second Ave	2P 830AM-6PM MON-FRI	6	4	0	1	0	0	4	3	3	4	5	5	5	5	4	6	5	4	6	5	6	5	5	6	4	4	3	4	4	3	3	
Waterview St	W	Between First Ave and Second Ave	Unrestricted	7	16	2	3	3	3	8	11	11	12	12	12	11	11	11	11	11	11	11	11	11	11	11	11	11	14	12	8	7	7	6	5
Waterview St	E	Between First Ave and Second Ave	Unrestricted	8	18	2	5	7	10	9	14	13	12	12	13	14	14	14	14	14	14	14	16	16	16	14	12	11	10	9	8	8	7	6	
Waterview St	W	Between Second Ave and Barnstaple Rd	Unrestricted	9	5	4	4	4	4	4	5	4	1	4	5	5	5	4	4	5	4	5	5	4	4	4	3	3	3	3	3	3	4	3	
Waterview St	W	Between Second Ave and Barnstaple Rd	2P 830AM-6PM MON-FRI	10	10	4	5	5	5	4	5	6	5	5	6	7	7	6	6	5	5	6	5	6	6	5	5	5	4	4	4	4	4	4	3
Waterview St	E	Between Second Ave and Barnstaple Rd	Unrestricted	11	14	9	9	9	10	8	9	11	12	13	13	14	13	12	14	12	12	13	11	12	12	12	12	13	12	11	9	10	10	10	
Total occupied					39	54	51	57	68	80	84	83	90	92	95	90	92	96	92	92	95	91	89	94	84	87	84	83	71	64	61	58	50		
Supply					0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	

Wednesday 13th October 2021					Date	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	
Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Great N Rd	W	Between East St Ped Crossing and St Alban's	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	1	15	21	18	24	25	24	20	21	18	22	20	17	15	18	18	20	17	18	19	20	17	19	20	17	18	16	18	14	18	9	
Second Ave	S	Between Great N Rd and Waterview St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	2	4	3	2	2	1	2	4	2	3	3	3	2	2	3	4	3	3	4	3	3	4	3	2	3	2	2	1	3	2	2	
Second Ave	N	Between Great N Rd and Waterview St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	4	1	1	2	1	3	2	2	1	2	2	2	1	2	3	2	1	2	1	2	1	2	2	2	3	1	2	2	1	1	
Second Ave	S	Between Waterview St and Sutton St	Unrestricted	4	7	2	2	2	2	3	4	5	5	5	6	5	5	5	5	5	5	5	5	5	5	5	5	5	6	5	6	6	6	6	
Second Ave	N	Between Waterview St and Sutton St	Unrestricted	5	10	0	1	1	3	6	7	8	8	8	8	8	8	8	9	8	9	10	8	8	8	8	7	4	5	3	5	5	5	5	
Waterview St	W	Between First Ave and Second Ave	2P 830AM-6PM MON-FRI	6	4	0	0	0	0	3	5	7	7	9	6	7	6	9	5	7	6	5	5	6	4	4	4	4	5	3	5	3	2	2	
Waterview St	W	Between First Ave and Second Ave	Unrestricted	7	16	7	7	7	8	10	11	12	11	12	13	14	12	13	14	12	12	13	13	13	12	11	11	11	8	8	8	8	8	7	
Waterview St	E	Between First Ave and Second Ave	Unrestricted	8	18	5	5	6	10	10	15	15	15	16	15	15	15	16	14	15	16	14	16	14	14	14	12	13	14	11	10	12	10	9	9
Waterview St	W	Between Second Ave and Barnstaple Rd	Unrestricted	9	5	3	3	3	3	4	4	5	5	4	5	5	5	4	4	4	5	5	4	4	5	4	4	4	4	4	3	4	5	4	
Waterview St	W	Between Second Ave and Barnstaple Rd	2P 830AM-6PM MON-FRI	10	10	3	4	4	4	4	4	5	4	5	5	5	4	5	5	5	6	7	5	5	6	5	5	5	5	4	5	4	3	4	
Waterview St	E	Between Second Ave and Barnstaple Rd	Unrestricted	11	14	9	10	10	10	9	10	12	12	12	13	10	14	10	12	13	12	13	12	11	11	12	10	11	9	8	9	8	9	9	
Total occupied					54	53	61	67	78	86	94	89	98	96	90	87	94	92	95	93	94	91	91	87	85	84	80	76	64	74	67	68	58		
Supply					0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	

Thursday 14th October 2021				Date	14/10/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																												
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Great N Rd	W	Between East St Ped Crossing and St Alban's	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	1	15	15	19	18	24	22	20	22	19	20	21	20	19	17	18	19	17	19	17	18	16	17	10	14	17	20	19	18	19	10
Second Ave	S	Between Great N Rd and Waterview St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	2	4	2	3	2	2	3	2	3	3	2	4	3	3	4	3	3	2	4	3	3	4	3	4	2	2	3	1	3	3	1
Second Ave	N	Between Great N Rd and Waterview St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	4	1	2	1	1	2	1	2	2	2	2	1	3	1	2	1	3	2	2	3	2	2	2	1	1	2	1	2	1	1
Second Ave	S	Between Waterview St and Sutton St	Unrestricted	4	7	3	3	3	3	3	6	6	5	6	6	7	5	6	6	6	5	6	6	5	5	5	2	1	2	3	4	3	3	3
Second Ave	N	Between Waterview St and Sutton St	Unrestricted	5	10	4	4	5	6	6	9	9	9	10	9	10	9	9	9	9	9	8	9	10	7	7	8	9	9	8	6	5	5	5
Waterview St	W	Between First Ave and Second Ave	2P 830AM-6PM MON-FRI	6	4	0	0	1	4	4	5	5	6	5	7	6	6	6	7	6	6	5	5	5	3	4	4	3	3	2	1	2	3	1
Waterview St	W	Between First Ave and Second Ave	Unrestricted	7	16	6	6	6	6	2	2	2	2	2	2	2	2	2	2	2	3	3	3	3	3	3	11	9	6	4	4	3	3	
Waterview St	E	Between First Ave and Second Ave	Unrestricted	8	18	5	5	6	6	10	10	10	10	10	11	10	10	11	11	10	10	11	11	10	8	9	13	15	11	8	8	7	7	7
Waterview St	W	Between Second Ave and Barnstaple Rd	Unrestricted	9	5	3	3	3	3	2	2	2	2	3	3	3	3	4	4	3	3	2	3	2	3	2	3	2	2	2	2	1	1	1
Waterview St	W	Between Second Ave and Barnstaple Rd	2P 830AM-6PM MON-FRI	10	10	4	5	5	4	4	6	5	4	5	6	6	6	6	7	5	4	5	4	4	3	4	4	4	4	4	4	4	4	3
Waterview St	E	Between Second Ave and Barnstaple Rd	Unrestricted	11	14	8	9	9	10	11	11	12	13	11	13	10	12	11	13	14	13	10	12	13	10	6	7	6	7	6	5	6	5	5
Total occupied					51	59	59	69	69	74	78	75	76	84	78	78	77	81	80	76	74	76	76	65	61	68	66	64	62	55	54	54	40	
Supply					0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	

Friday 15th October 2021				Date	15/10/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																												
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Great N Rd	W	Between East St Ped Crossing and St Alban's	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	1	15	15	13	14	15	19	15	15	12	14	16	19	18	15	19	16	18	17	19	16	18	17	16	14	16	17	19	16	14	14
Second Ave	S	Between Great N Rd and Waterview St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	2	4	1	2	2	3	4	2	2	3	4	3	4	3	2	4	3	4	3	4	3	3	4	3	3	4	3	4	3	3	
Second Ave	N	Between Great N Rd and Waterview St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	4	2	1	2	1	3	1	1	2	3	2	2	3	2	2	2	1	2	2	1	2	1	2	1	2	2	1	2	1	2
Second Ave	S	Between Waterview St and Sutton St	Unrestricted	4	7	3	3	3	2	4	5	5	5	5	5	5	7	6	6	6	6	6	6	6	5	5	4	5	4	6	5	3	3	
Second Ave	N	Between Waterview St and Sutton St	Unrestricted	5	10	3	4	5	9	7	10	9	9	9	9	9	9	9	10	9	10	10	10	9	9	9	9	7	6	6	5	2	2	2
Waterview St	W	Between First Ave and Second Ave	2P 830AM-6PM MON-FRI	6	4	0	0	1	1	2	3	3	4	4	4	3	5	5	4	7	5	4	4	3	3	5	5	4	3	4	2	2	2	
Waterview St	W	Between First Ave and Second Ave	Unrestricted	7	16	5	6	6	5	10	11	12	13	13	12	14	12	12	12	12	13	12	12	12	12	10	12	10	10	10	10	11	8	8
Waterview St	E	Between First Ave and Second Ave	Unrestricted	8	18	6	6	7	7	9	10	11	11	9	9	9	9	10	9	10	10	11	10	10	9	9	11	12	11	11	12	11	10	10
Waterview St	W	Between Second Ave and Barnstaple Rd	Unrestricted	9	5	1	1	1	2	3	3	3	3	3	3	3	3	3	3	3	3	4	3	3	3	2	2	3	2	2	2	2	2	
Waterview St	W	Between Second Ave and Barnstaple Rd	2P 830AM-6PM MON-FRI	10	10	2	3	3	4	4	5	4	5	6	5	5	4	5	5	4	4	4	4	5	4	4	5	3	4	2	3	2	2	2
Waterview St	E	Between Second Ave and Barnstaple Rd	Unrestricted	11	14	4	4	6	7	7	5	10	10	9	8	9	8	9	8	9	8	10	9	10	7	8	9	9	9	9	9	9	8	8
Total occupied					42	43	50	56	72	70	75	77	79	76	82	81	78	81	82	81	82	84	78	76	74	80	69	72	71	74	66	55	56	
Supply					0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	

TRANS TRAFFIC SURVEY

trafficsurvey.com.au



Map and Surveyed Area

Suburb:

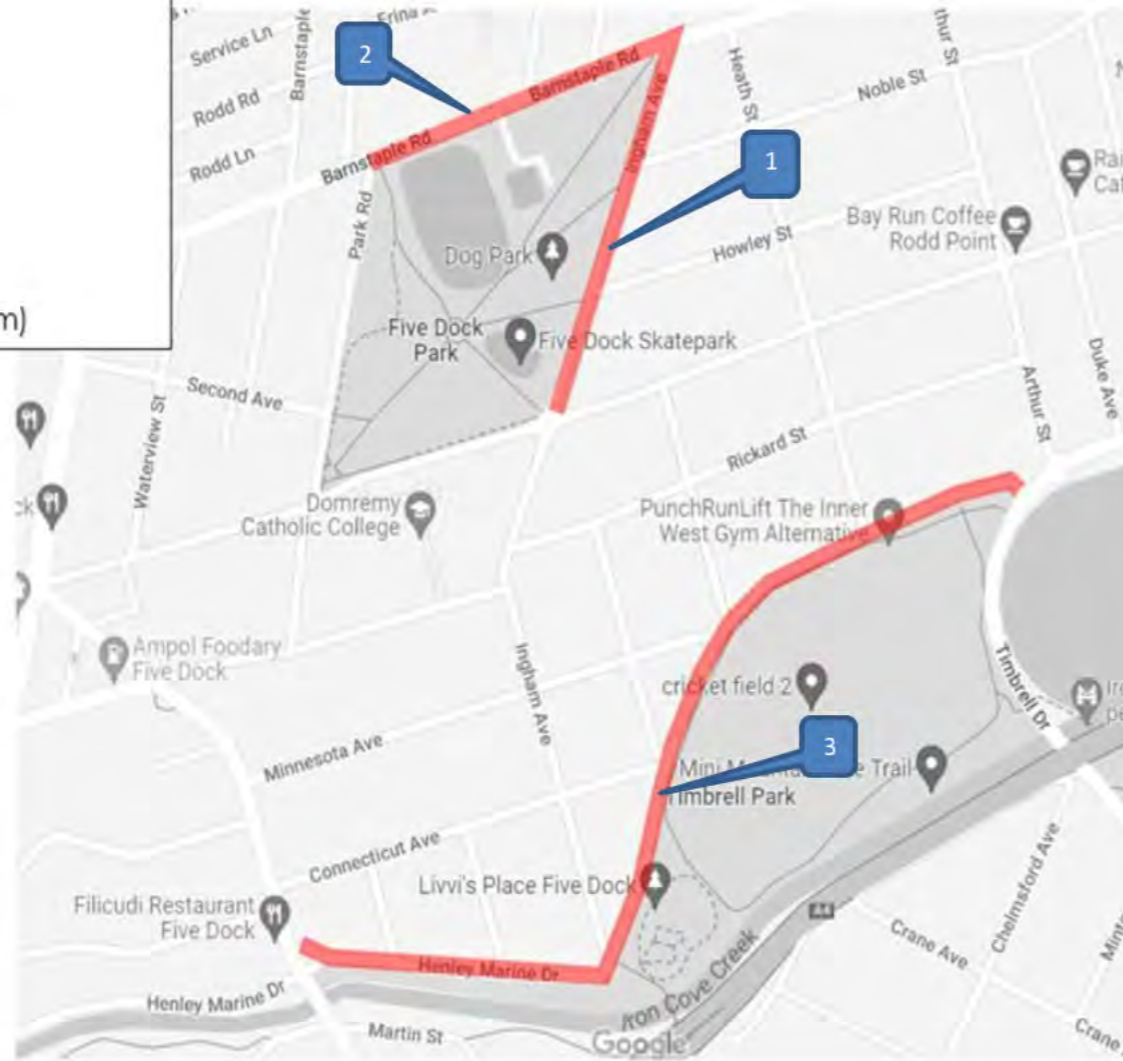
- Five Dock

Streets:



- Henley Marine Drive
- Barnstaple Road
- Ingham Avenue

Time/Duration:

- 7 days (6:00am – 8:00pm)



Legend

-  Location of survey
-  Map reference



Parking Occupancy Survey

Date:	Saturday, 7 May 2022
Location:	Barnstaple Rd, Five Dock
GPS:	-33.864646, 151 135706
Weather:	Fine
Customer:	TTPP

Public Parking (1/0)	Map Ref	Street	Section	Side	Restriction	Clear Way	Capacity	Parking Occupancy															
								6:00	7:00	8:00	9:00	10:00	11:00	12:00	13:00	14:00	15:00	16:00	17:00	18:00	19:00	20:00	
0	1	Barnstaple Rd	Trevanion St To Bennett Ave	N	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			Trevanion St To Bennett Ave	N	Unrestricted		20	6	7	8	8	7	6	6	6	7	7	7	7	9	9	10	
1			Bennett Ave To Betts Ave	N	Unrestricted		6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1				N	Unrestricted		6	0	0	0	0	0	0	1	1	1	1	1	1	1	1	1	1
1			Ingham Ave To Park Rd	S	Unrestricted		12	1	1	1	1	1	1	1	2	2	2	2	1	1	1	1	1
1			Ingham Ave To Park Rd - East	S	Unrestricted - 90 Degree Parking		12	3	3	5	6	6	6	6	3	3	3	3	3	2	1	2	2
1			Ingham Ave To Park Rd	S	Disabled Parking		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			Ingham Ave To Park Rd - West	S	Unrestricted - 90 Degree Parking		16	3	3	4	2	3	3	0	0	2	3	3	2	2	1	2	2
1			Ingham Ave To Park Rd	S	Unrestricted		21	8	7	6	6	6	8	4	4	4	4	5	8	8	8	8	9
1			2	Ingham Ave	Barnstaple Rd To Noble St	E	Unrestricted		12	4	5	5	5	6	7	8	8	5	4	5	5	4	5
1	Noble St To Howley St	E			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
		E			Unrestricted		10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1	Howley St To First Ave	E			Unrestricted		11	4	4	5	5	5	5	5	5	5	5	5	5	5	5	5	5
1	First Ave To Barnstaple Rd	W			Unrestricted		12	6	7	8	8	8	7	7	7	7	7	6	5	6	6	6	6
0		W			No Stopping		4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1		W			Unrestricted		15	8	8	8	8	7	8	8	8	8	8	7	8	9	9	9	9
1		W			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1	1	W	Unrestricted		13	5	4	5	5	5	5	6	6	6	5	5	5	4	5	5	5		
1	3	Henley Marine Dr	Ramsay Rd To Illinois Rd	N	Unrestricted		9	6	7	7	7	8	9	9	9	8	7	5	8	4	5	6	
1			Illinois Rd To New Jersey Rd	N	Unrestricted		12	7	8	8	8	7	8	5	4	8	8	8	7	5	8	4	
1			New Jersey Rd To Ingham Ave	N	Unrestricted		10	5	6	6	6	4	7	8	8	8	9	9	9	6	5	6	6
1			Ingham Ave To Connecticut Ave	W	Unrestricted		24	12	13	18	19	18	18	17	17	17	16	15	17	18	12	9	9
1			Norman St To Minnesota Ave	W	Unrestricted		13	5	8	8	7	7	7	8	8	8	9	9	9	8	7	8	8
1			Augusta St To Helth St	N	Unrestricted		14	2	3	4	4	5	2	7	8	8	9	9	9	7	8	4	4
1			Helth St To Timbrell Dr	N	Unrestricted		20	12	14	15	15	15	15	16	16	14	12	12	12	11	12	12	12
1			S/E/S	Unrestricted		6	4	4	3	6	6	6	6	6	6	6	6	6	6	6	6	5	5
0			S/E/S	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Y2.1 - for worker parking area 21319-Five Dock On-Street Parking-220525

1			S/E/S	Disabled Parking		1	0	0	0	0	1	1	1	1	1	1	0	0	0	0	
1			S/E/S	Unrestricted		77	32	30	30	30	29	41	41	39	38	39	40	40	40	36	24
0			S/E/S	No Stopping		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1		Timbrell Dr To Ramsay Rd	S/E/S	Unrestricted		5	4	2	3	3	4	4	4	2	2	5	5	2	2	4	5
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			S/E/S	Disabled Parking		2	0	0	0	0	0	1	1	2	2	1	1	2	1	0	0
1			S/E/S	Unrestricted		49	25	26	26	26	31	36	35	36	41	37	36	34	26	24	22
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PUBLIC CAPACITY							405	405	405	405	405	405	405	405	405	405	405	405	405	405	405
PUBLIC OCCUPANCIES							162	170	183	185	189	211	210	206	211	208	205	205	185	178	160
PUBLIC VACANCIES							243	235	222	220	216	194	195	199	194	197	200	200	220	227	245
PUBLIC % OCCUPANCIES							40%	42%	45%	46%	47%	52%	52%	51%	52%	51%	51%	51%	46%	44%	40%

not available for public parking

Parking Occupancy Survey

Date:	Sunday, 8 May 2022
Location:	Barnstaple Rd, Five Dock
GPS:	-33.864646, 151 135706
Weather:	Fine
Customer:	TTPP

Public Parking (1/0)	Map Ref	Street	Section	Side	Restriction	Clear Way	Capacity	Parking Occupancy															
								6:00	7:00	8:00	9:00	10:00	11:00	12:00	13:00	14:00	15:00	16:00	17:00	18:00	19:00	20:00	
0	1	Barnstaple Rd	Trevanion St To Bennett Ave	N	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			Trevanion St To Bennett Ave	N	Unrestricted		20	6	7	7	5	7	6	7	7	6	5	5	6	9	9	9	
1			Bennett Ave To Betts Ave	N	Unrestricted		6	0	0	0	0	0	1	1	1	1	0	0	0	0	0	0	
1				N	Unrestricted		6	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1
1			Ingham Ave To Park Rd	S	Unrestricted		12	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1
1			Ingham Ave To Park Rd - East	S	Unrestricted - 90 Degree Parking		12	2	3	3	3	3	2	2	2	2	1	2	2	2	3	1	
1			Ingham Ave To Park Rd	S	Disabled Parking		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			Ingham Ave To Park Rd - West	S	Unrestricted - 90 Degree Parking		16	2	2	2	3	2	3	3	3	2	2	2	2	1	2	2	
1			Ingham Ave To Park Rd	S	Unrestricted		21	6	7	8	8	8	7	8	8	8	8	9	9	10	10	11	
1			2	Ingham Ave	Barnstaple Rd To Noble St	E	Unrestricted		12	5	4	5	5	5	6	6	6	6	5	4	5	5	6
1	Noble St To Howley St	E			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
		E			Unrestricted		10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1	Howley St To First Ave	E			Unrestricted		11	5	5	5	4	5	5	5	5	5	6	6	6	3	2	1	
1	First Ave To Barnstaple Rd	W			Unrestricted		12	6	8	8	8	7	8	9	9	9	2	3	4	2	2	2	
0		W			No Stopping		4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1		W			Unrestricted		15	4	5	5	6	8	7	8	9	9	9	4	7	4	5	2	
1		W			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			W	Unrestricted		13	3	4	5	5	5	6	8	7	8	8	8	5	4	5	5		
1	3	Henley Marine Dr	Ramsay Rd To Illinois Rd	N	Unrestricted		9	9	9	8	7	8	9	9	8	7	8	9	5	4	5	6	
1			Illinois Rd To New Jersey Rd	N	Unrestricted		12	8	7	7	7	8	5	5	5	8	7	8	5	4	8	7	
1			New Jersey Rd To Ingham Ave	N	Unrestricted		10	6	6	6	6	7	7	7	8	8	7	5	4	5	5	6	
1			Ingham Ave To Connecticut Ave	W	Unrestricted		24	9	9	12	14	15	16	15	15	14	18	18	12	12	13	12	
1			Norman St To Minnesota Ave	W	Unrestricted		13	8	8	8	8	8	7	8	8	8	7	8	8	6	5	4	
1			Augusta St To Helth St	N	Unrestricted		14	4	5	6	5	8	7	8	8	9	9	9	8	8	7	7	
1			Helth St To Timbrell Dr	N	Unrestricted		20	13	13	13	12	12	12	12	13	12	12	14	12	12	13	11	
1				S/E/S	Unrestricted		6	4	4	3	5	4	5	6	6	6	6	6	6	6	4	3	
0				S/E/S	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Y2.1 - for worker parking area 21319-Five Dock On-Street Parking-220525

1			S/E/S	Disabled Parking		1	0	0	0	1	1	1	1	1	1	0	0	0	0	1	0
1			S/E/S	Unrestricted		77	27	29	32	33	35	41	41	43	38	37	38	36	33	34	32
0			S/E/S	No Stopping		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1		Timbrell Dr To Ramsay Rd	S/E/S	Unrestricted		5	2	3	3	3	3	2	4	5	5	2	3	2	5	4	2
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			S/E/S	Disabled Parking		2	0	0	0	0	0	1	1	1	2	2	2	1	0	0	0
1			S/E/S	Unrestricted		49	23	23	23	23	36	36	41	39	39	34	36	28	27	24	26
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PUBLIC CAPACITY							405	405	405	405	405	405	405	405	405	405	405	405	405	405	405
PUBLIC OCCUPANCIES							154	163	171	173	197	202	217	219	215	198	202	174	164	168	157
PUBLIC VACANCIES							251	242	234	232	208	203	188	186	190	207	203	231	241	237	248
PUBLIC % OCCUPANCIES							38%	40%	42%	43%	49%	50%	54%	54%	53%	49%	50%	43%	40%	41%	39%

not available for public parking



Parking Occupancy Survey

Date:	Monday, 9 May 2022
Location:	Barnstaple Rd, Five Dock
GPS:	-33.864646, 151 135706
Weather:	Fine
Customer:	TTPP

Public Parking (1/0)	Map Ref	Street	Section	Side	Restriction	Clear Way	Capacity	Parking Occupancy																
								6:00	7:00	8:00	9:00	10:00	11:00	12:00	13:00	14:00	15:00	16:00	17:00	18:00	19:00	20:00		
0	1	Barnstaple Rd	Trevanion St To Bennett Ave	N	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			Trevanion St To Bennett Ave	N	Unrestricted		20	6	5	6	6	6	5	5	5	6	8	7	7	6	6	7		
1			Bennett Ave To Betts Ave	N	Unrestricted		6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1				N	Unrestricted		6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			Ingham Ave To Park Rd	S	Unrestricted		12	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1
1			Ingham Ave To Park Rd - East	S	Unrestricted - 90 Degree Parking		12	2	3	3	3	2	3	3	3	3	2	3	3	2	2	2	2	
1			Ingham Ave To Park Rd	S	Disabled Parking		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			Ingham Ave To Park Rd - West	S	Unrestricted - 90 Degree Parking		16	2	3	3	3	2	1	2	2	3	4	2	3	3	3	3	3	
1			Ingham Ave To Park Rd	S	Unrestricted		21	8	9	5	5	6	4	5	5	5	7	8	8	8	9	9	9	
1			2	Ingham Ave	Barnstaple Rd To Noble St	E	Unrestricted		12	4	4	4	4	5	6	6	5	6	6	6	6	5	4	5
1	Noble St To Howley St	E			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
		E			Unrestricted		10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1	Howley St To First Ave	E			Unrestricted		11	2	3	4	4	5	6	5	4	5	6	5	4	6	5	3		
1	First Ave To Barnstaple Rd	W			Unrestricted		12	2	5	3	3	6	5	4	1	2	2	3	6	5	4	2		
0		W			No Stopping		4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1		W			Unrestricted		15	3	3	5	4	5	5	6	5	4	4	4	4	4	4	4	4	
1		W			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1	W	Unrestricted		13	4	5	6	6	6	6	6	6	5	4	5	5	6	6	6	6	6			
1	3	Henley Marine Dr	Ramsay Rd To Illinois Rd	N	Unrestricted		9	5	5	5	6	4	8	8	8	8	7	8	8	9	8	4		
1			Illinois Rd To New Jersey Rd	N	Unrestricted		12	8	8	8	9	9	9	9	8	8	8	7	8	8	6	8		
1			New Jersey Rd To Ingham Ave	N	Unrestricted		10	6	6	7	8	9	5	4	5	5	5	4	4	3	5	4		
1			Ingham Ave To Connecticut Ave	W	Unrestricted		24	12	12	12	13	13	14	15	15	14	14	4	13	13	12	13		
1			Norman St To Minnesota Ave	W	Unrestricted		13	4	4	4	5	3	6	5	4	5	5	5	5	4	5	5		
1			Augusta St To Helth St	N	Unrestricted		14	4	4	4	2	3	4	4	4	4	2	3	3	3	4	2		
1			Helth St To Timbrell Dr	N	Unrestricted		20	12	13	14	12	15	14	15	13	14	12	12	12	13	12	12		
1			S/E/S	Unrestricted		6	4	4	5	3	2	5	4	6	6	6	6	6	4	5	4			
0			S/E/S	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	



Parking Occupancy Survey

Date:	Tuesday, 10 May 2022
Location:	Barnstaple Rd, Five Dock
GPS:	-33.864646, 151 135706
Weather:	Fine
Customer:	TTPP

Public Parking (1/0)	Map Ref	Street	Section	Side	Restriction	Clear Way	Capacity	Parking Occupancy															
								6:00	7:00	8:00	9:00	10:00	11:00	12:00	13:00	14:00	15:00	16:00	17:00	18:00	19:00	20:00	
0	1	Barnstaple Rd	Trevanion St To Bennett Ave	N	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			Trevanion St To Bennett Ave	N	Unrestricted		20	6	6	6	5	5	4	5	6	5	3	5	9	10	11	11	
1			Bennett Ave To Betts Ave	N	Unrestricted		6	0	0	0	0	0	0	0	0	0	0	0	0	2	2	2	
1				N	Unrestricted		6	0	0	0	1	1	1	1	1	1	1	1	1	1	1	1	1
1			Ingham Ave To Park Rd	S	Unrestricted		12	1	1	1	1	1	2	2	2	1	2	2	1	1	1	1	1
1			Ingham Ave To Park Rd - East	S	Unrestricted - 90 Degree Parking		12	4	4	4	4	5	4	4	4	5	5	7	5	6	4	3	
1			Ingham Ave To Park Rd	S	Disabled Parking		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			Ingham Ave To Park Rd - West	S	Unrestricted - 90 Degree Parking		16	3	4	2	3	3	4	4	4	3	5	4	5	6	5	5	
1			Ingham Ave To Park Rd	S	Unrestricted		21	4	5	3	5	4	5	4	5	2	3	4	9	12	16	14	
1			2	Ingham Ave	Barnstaple Rd To Noble St	E	Unrestricted		12	5	7	8	8	9	5	5	4	5	6	3	3	4	5
1	Noble St To Howley St	E			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
		E			Unrestricted		10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1	Howley St To First Ave	E			Unrestricted		11	4	5	5	5	4	5	5	5	5	5	5	4	5	5	5	
1	First Ave To Barnstaple Rd	W			Unrestricted		12	3	2	3	4	5	4	5	5	4	5	6	2	3	1	2	
0		W			No Stopping		4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1		W			Unrestricted		15	4	4	4	4	4	4	5	5	5	5	5	4	4	4	4	
1		W			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1	W	Unrestricted		13	6	6	6	6	6	5	4	5	5	5	5	5	5	5	4	5			
1	3	Henley Marine Dr	Ramsay Rd To Illinois Rd	N	Unrestricted		9	4	5	5	9	8	7	8	9	9	8	7	8	8	6	5	
1			Illinois Rd To New Jersey Rd	N	Unrestricted		12	7	8	8	8	7	8	8	8	7	8	6	5	4	5	5	
1			New Jersey Rd To Ingham Ave	N	Unrestricted		10	5	5	6	6	5	5	4	4	4	4	4	5	4	5	5	
1			Ingham Ave To Connecticut Ave	W	Unrestricted		24	12	11	11	11	11	13	12	11	11	12	12	12	10	10	10	
1			Norman St To Minnesota Ave	W	Unrestricted		13	5	6	4	5	5	5	5	5	4	4	5	5	5	5	5	
1			Augusta St To Helth St	N	Unrestricted		14	0	0	0	2	4	2	3	3	3	2	4	2	2	3	3	
1			Helth St To Timbrell Dr	N	Unrestricted		20	8	9	12	10	11	11	10	10	10	10	11	9	9	8	7	
1			S/E/S	Unrestricted		6	2	3	3	3	4	4	4	5	6	6	5	4	6	6	5		
0			S/E/S	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	

Y2.1 - for worker parking area 21319-Five Dock On-Street Parking-220525

1			S/E/S	Disabled Parking		1	0	0	0	0	0	0	0	0	0	0	1	1	1	1	0	0	0
1			S/E/S	Unrestricted		77	13	13	12	13	15	19	22	24	25	25	22	24	25	22	20		
0			S/E/S	No Stopping		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1		Timbrell Dr To Ramsay Rd	S/E/S	Unrestricted		5	2	2	2	2	2	2	2	2	2	2	3	3	3	4	3	3	
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			S/E/S	Disabled Parking		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			S/E/S	Unrestricted		49	24	24	24	23	23	23	24	25	26	26	26	24	25	25	23		
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PUBLIC CAPACITY							405	405	405	405	405	405	405	405	405	405	405	405	405	405	405	405	405
PUBLIC OCCUPANCIES							122	130	129	138	142	142	146	152	149	154	153	150	161	157	149		
PUBLIC VACANCIES							283	275	276	267	263	263	259	253	256	251	252	255	244	248	256		
PUBLIC % OCCUPANCIES							30%	32%	32%	34%	35%	35%	36%	38%	37%	38%	38%	37%	40%	39%	37%		

not available for public parking



Parking Occupancy Survey

Date:	Wednesday, 11 May 2022
Location:	Barnstaple Rd, Five Dock
GPS:	-33.864646, 151 135706
Weather:	Fine
Customer:	TTPP

Public Parking (1/0)	Map Ref	Street	Section	Side	Restriction	Clear Way	Capacity	Parking Occupancy																
								6:00	7:00	8:00	9:00	10:00	11:00	12:00	13:00	14:00	15:00	16:00	17:00	18:00	19:00	20:00		
0	1	Barnstaple Rd	Trevanion St To Bennett Ave	N	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
1			Trevanion St To Bennett Ave	N	Unrestricted		20	3	4	5	4	6	5	6	4	6	8	8	7	9	7	8		
1			Bennett Ave To Betts Ave	N	Unrestricted		6	0	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	
1				N	Unrestricted		6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			Ingham Ave To Park Rd	S	Unrestricted		12	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	
1			Ingham Ave To Park Rd - East	S	Unrestricted - 90 Degree Parking		12	3	5	5	4	4	3	5	5	4	5	6	6	5	4	4	4	
1			Ingham Ave To Park Rd	S	Disabled Parking		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			Ingham Ave To Park Rd - West	S	Unrestricted - 90 Degree Parking		16	3	3	4	4	4	5	4	5	5	4	4	4	6	5	5	5	
1			Ingham Ave To Park Rd	S	Unrestricted		21	6	4	5	5	5	4	4	5	4	5	6	6	4	4	3	3	
1			2	Ingham Ave	Barnstaple Rd To Noble St	E	Unrestricted		12	6	5	4	5	5	5	5	4	5	5	4	5	5	5	5
1	Noble St To Howley St	E			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
		E			Unrestricted		10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1	Howley St To First Ave	E			Unrestricted		11	5	6	4	8	4	5	5	5	5	8	5	7	7	7	6	6	
1	First Ave To Barnstaple Rd	W			Unrestricted		12	4	5	5	4	5	6	5	4	8	8	9	9	6	5	4	4	
0		W			No Stopping		4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1		W			Unrestricted		15	8	7	8	9	9	9	9	7	8	9	6	5	3	2	1	1	1
1		W			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1	1	W	Unrestricted		13	2	4	3	6	5	4	5	5	5	5	5	5	6	6	6	6	6		
1	3	Henley Marine Dr	Ramsay Rd To Illinois Rd	N	Unrestricted		9	4	5	6	5	5	4	8	4	7	5	5	4	8	4	5		
1			Illinois Rd To New Jersey Rd	N	Unrestricted		12	5	5	4	5	5	5	5	6	7	8	8	8	7	8	4	4	
1			New Jersey Rd To Ingham Ave	N	Unrestricted		10	4	5	5	5	5	5	5	6	4	4	5	6	7	5	5	5	
1			Ingham Ave To Connecticut Ave	W	Unrestricted		24	8	8	8	7	8	9	9	9	5	6	4	8	8	8	8	8	8
1			Norman St To Minnesota Ave	W	Unrestricted		13	4	5	5	5	5	5	5	5	5	4	4	4	4	6	6	6	6
1			Augusta St To Helth St	N	Unrestricted		14	3	3	3	2	2	2	2	3	3	3	3	4	4	4	4	4	4
1			Helth St To Timbrell Dr	N	Unrestricted		20	8	8	8	8	9	9	9	12	10	10	11	11	14	10	9	9	9
1				S/E/S	Unrestricted		6	5	4	5	5	5	2	3	3	3	2	1	2	3	2	3	3	3
0				S/E/S	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Y2.1 - for worker parking area 21319-Five Dock On-Street Parking-220525

1			S/E/S	Disabled Parking		1	0	0	0	0	0	0	0	0	0	0	0	0	0			
1			S/E/S	Unrestricted		77	17	17	16	17	16	17	21	22	23	23	19	18	16	17	11	
0			S/E/S	No Stopping		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
1		Timbrell Dr To Ramsay Rd	S/E/S	Unrestricted		5	2	2	2	2	2	3	3	3	3	3	3	3	4	2	2	
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			S/E/S	Disabled Parking		2	0	0	0	0	0	0	0	0	0	0	1	1	1	0	0	0
1			S/E/S	Unrestricted		49	24	24	25	25	25	25	25	25	25	25	25	25	23	23	23	27
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PUBLIC CAPACITY							405	405	405	405	405	405	405	405	405	405	405	405	405	405	405	
PUBLIC OCCUPANCIES							125	130	131	136	135	133	145	145	146	152	144	146	150	135	127	
PUBLIC VACANCIES							280	275	274	269	270	272	260	260	259	253	261	259	255	270	278	
PUBLIC % OCCUPANCIES							31%	32%	32%	34%	33%	33%	36%	36%	36%	38%	36%	36%	37%	33%	31%	

not available for public parking



Parking Occupancy Survey

Date:	Thursday, 12 May 2022
Location:	Barnstaple Rd, Five Dock
GPS:	-33.864646, 151 135706
Weather:	Fine
Customer:	TTPP

Public Parking (1/0)	Map Ref	Street	Section	Side	Restriction	Clear Way	Capacity	Parking Occupancy														
								6:00	7:00	8:00	9:00	10:00	11:00	12:00	13:00	14:00	15:00	16:00	17:00	18:00	19:00	20:00
0	1	Barnstaple Rd	Trevanion St To Bennett Ave	N	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			Trevanion St To Bennett Ave	N	Unrestricted		20	5	4	3	5	5	5	6	5	5	4	4	6	7	6	7
1			Bennett Ave To Betts Ave	N	Unrestricted		6	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0
1				N	Unrestricted		6	0	0	1	0	0	0	1	1	1	1	1	1	1	1	1
1			Ingham Ave To Park Rd	S	Unrestricted		12	1	1	1	1	1	1	1	1	1	2	2	2	1	1	1
1			Ingham Ave To Park Rd - East	S	Unrestricted - 90 Degree Parking		12	2	3	3	3	1	4	4	4	2	3	3	3	4	4	4
1			Ingham Ave To Park Rd	S	Disabled Parking		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			Ingham Ave To Park Rd - West	S	Unrestricted - 90 Degree Parking		16	3	3	3	4	3	3	3	3	3	3	4	4	4	4	4
1			Ingham Ave To Park Rd	S	Unrestricted		21	5	6	4	6	6	5	6	6	6	6	5	4	5	5	5
1			2	Ingham Ave	Barnstaple Rd To Noble St	E	Unrestricted		12	5	4	5	5	5	6	3	2	5	4	1	2	2
1	Noble St To Howley St	E			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
		E			Unrestricted		10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1	Howley St To First Ave	E			Unrestricted		11	4	4	4	5	6	5	4	4	4	4	4	5	6	6	2
1	First Ave To Barnstaple Rd	W			Unrestricted		12	5	2	3	4	5	5	6	5	4	2	3	5	3	3	2
0		W			No Stopping		4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1		W			Unrestricted		15	2	5	4	4	4	2	3	6	5	1	2	2	2	5	2
1		W			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1	1	W	Unrestricted		13	6	6	6	6	6	5	5	5	5	4	5	6	5	6	6		
1	3	Henley Marine Dr	Ramsay Rd To Illinois Rd	N	Unrestricted		9	7	7	7	7	8	9	9	9	8	7	8	5	7	4	5
1			Illinois Rd To New Jersey Rd	N	Unrestricted		12	5	5	5	5	5	4	5	5	5	5	8	7	8	8	8
1			New Jersey Rd To Ingham Ave	N	Unrestricted		10	6	6	6	6	6	6	5	4	5	5	4	4	5	4	5
1			Ingham Ave To Connecticut Ave	W	Unrestricted		24	9	9	9	8	8	5	6	4	5	4	7	7	7	6	4
1			Norman St To Minnesota Ave	W	Unrestricted		13	6	6	6	6	6	6	6	6	5	5	5	4	5	5	5
1			Augusta St To Helth St	N	Unrestricted		14	4	4	4	4	4	4	3	2	2	3	3	4	4	5	4
1			Helth St To Timbrell Dr	N	Unrestricted		20	12	13	13	11	12	12	12	10	10	10	12	11	11	12	12
1			S/E/S	Unrestricted		6	4	5	5	5	4	5	1	2	2	3	2	1	1	2	1	
0			S/E/S	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Y2.1 - for worker parking area 21319-Five Dock On-Street Parking-220525

1			S/E/S	Disabled Parking		1	0	0	0	0	0	0	0	0	0	0	1	1	1	1	0	0	0
1			S/E/S	Unrestricted		77	23	23	24	22	19	20	17	18	17	18	18	15	11	9	10		
0			S/E/S	No Stopping		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1		Timbrell Dr To Ramsay Rd	S/E/S	Unrestricted		5	2	3	3	3	3	3	3	2	4	2	3	2	4	2	2		
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			S/E/S	Disabled Parking		2	0	0	0	0	0	0	1	1	1	1	1	1	1	0	0	0	0
1			S/E/S	Unrestricted		49	24	25	25	25	26	26	23	31	31	32	30	24	25	21	22		
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PUBLIC CAPACITY							405	405	405	405	405	405	405	405	405	405	405	405	405	405	405	405	405
PUBLIC OCCUPANCIES							140	144	144	146	144	142	133	136	137	130	136	126	128	122	114		
PUBLIC VACANCIES							265	261	261	259	261	263	272	269	268	275	269	279	277	283	291		
PUBLIC % OCCUPANCIES							35%	36%	36%	36%	36%	35%	33%	34%	34%	32%	34%	31%	32%	30%	28%		

not available for public parking



Parking Occupancy Survey

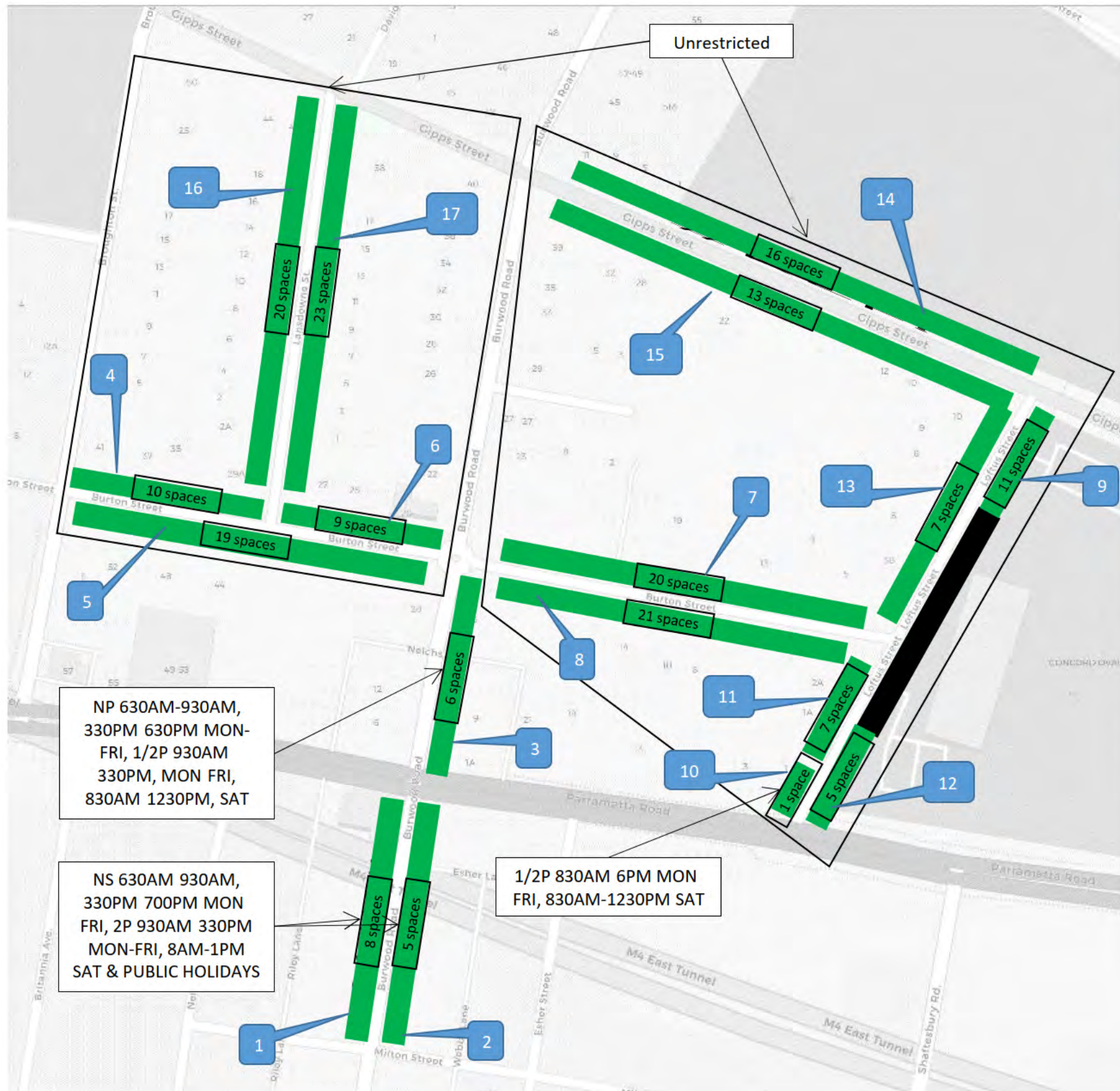
Date:	Friday, 13 May 2022
Location:	Barnstaple Rd, Five Dock
GPS:	-33.864646, 151 135706
Weather:	Fine
Customer:	TTPP

Public Parking (1/0)	Map Ref	Street	Section	Side	Restriction	Clear Way	Capacity	Parking Occupancy																
								6:00	7:00	8:00	9:00	10:00	11:00	12:00	13:00	14:00	15:00	16:00	17:00	18:00	19:00	20:00		
0	1	Barnstaple Rd	Trevanion St To Bennett Ave	N	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
1			Trevanion St To Bennett Ave	N	Unrestricted		20	5	5	5	6	7	7	6	5	5	9	10	10	12	11	12		
1			Bennett Ave To Betts Ave	N	Unrestricted		6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			Bennett Ave To Betts Ave	N	Unrestricted		6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1			Ingham Ave To Park Rd	S	Unrestricted		12	1	1	1	1	1	1	1	2	2	2	2	1	1	1	1	1	
1			Ingham Ave To Park Rd - East	S	Unrestricted - 90 Degree Parking		12	4	4	4	3	4	4	4	5	5	5	6	6	6	5	5	5	
1			Ingham Ave To Park Rd	S	Disabled Parking		1	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	0	
1			Ingham Ave To Park Rd - West	S	Unrestricted - 90 Degree Parking		16	3	3	3	3	3	4	4	4	4	5	5	3	3	3	3	3	
1			Ingham Ave To Park Rd	S	Unrestricted		21	4	5	5	5	6	5	4	5	6	8	9	9	10	12	10	10	
1			2	Ingham Ave	Barnstaple Rd To Noble St	E	Unrestricted		12	1	2	3	6	5	4	3	3	3	2	4	2	2	3	4
1	Noble St To Howley St	E			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1	Noble St To Howley St	E			Unrestricted		10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
1	Howley St To First Ave	E			Unrestricted		11	5	7	8	8	9	9	9	5	4	5	5	5	5	4	5	5	
0	First Ave To Barnstaple Rd	W			Unrestricted		12	3	3	3	2	4	2	3	5	4	5	5	5	5	5	6	6	
1	First Ave To Barnstaple Rd	W			No Stopping		4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1	First Ave To Barnstaple Rd	W			Unrestricted		15	3	5	7	8	9	6	6	6	6	5	4	5	5	5	5	5	
1	First Ave To Barnstaple Rd	W			Bus Zone		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1	3	Henley Marine Dr	Ramsay Rd To Illinois Rd	N	Unrestricted		9	5	4	5	5	5	7	8	8	8	9	9	5	4	5	6		
1			Illinois Rd To New Jersey Rd	N	Unrestricted		12	5	4	4	5	6	4	7	4	5	5	6	6	5	4	7		
1			New Jersey Rd To Ingham Ave	N	Unrestricted		10	5	5	5	5	5	5	4	4	4	4	6	7	5	5	4		
1			Ingham Ave To Connecticut Ave	W	Unrestricted		24	8	7	8	9	5	9	8	8	7	7	8	8	8	8	9	10	
1			Norman St To Minnesota Ave	W	Unrestricted		13	4	5	5	5	5	5	5	4	4	5	5	5	5	5	5	5	
1			Augusta St To Helth St	N	Unrestricted		14	5	5	4	5	5	5	5	5	5	4	2	3	3	4	2		
1			Helth St To Timbrell Dr	N	Unrestricted		20	9	9	7	8	9	10	12	12	11	13	11	12	10	11	11		
1			Henley Marine Dr	S/E/S	Unrestricted		6	4	4	5	5	6	5	4	5	2	1	2	2	1	2	2	2	
0			Henley Marine Dr	S/E/S	No Stopping		2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			Henley Marine Dr	S/E/S	Disabled Parking		1	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0

Y2.1 - for worker parking area 21319-Five Dock On-Street Parking-220525

1			S/E/S	Unrestricted		77	16	13	13	14	12	15	14	17	18	17	20	17	21	19	20
0			S/E/S	No Stopping		1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1		Timbrell Dr To Ramsay Rd	S/E/S	Unrestricted		5	2	3	3	3	3	3	3	3	3	3	2	4	2	2	2
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1			S/E/S	Disabled Parking		2	0	0	0	0	0	0	0	0	0	1	1	1	0	0	0
1			S/E/S	Unrestricted		49	22	23	24	25	25	25	29	31	31	32	29	27	25	25	24
0			S/E/S	No Parking		3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PUBLIC CAPACITY							405	405	405	405	405	405	405	405	405	405	405	405	405	405	405
PUBLIC OCCUPANCIES							119	122	126	135	138	140	145	147	142	154	157	147	142	146	150
PUBLIC VACANCIES							286	283	279	270	267	265	260	258	263	251	248	258	263	259	255
PUBLIC % OCCUPANCIES							29%	30%	31%	33%	34%	35%	36%	36%	35%	38%	39%	36%	35%	36%	37%

not available for public parking



Legend
Location of survey
Map reference

Friday 10th September 2021				Date		10/09/2021																												
Street	Side	Area	Restriction	Map reference	Supply	Time																												
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Burwood Road	W	Between Milton St and Parramatta Rd	NS 630AM-930AM, 330PM-700PM MON-FRI, 2P 930AM-330PM MON-FRI, 8AM-1PM	1	8	0	0	0	0	0	0	0	0	2	3	5	7	2	5	6	3	5	4	2	2	0	4	0	0	6	5	5	5	4
Burwood Road	E	Between Milton St and Parramatta Rd	NS 630AM-930AM, 330PM-700PM MON-FRI, 2P 930AM-330PM MON-FRI, 8AM-1PM	2	5	1	1	1	1	1	1	1	2	2	3	1	2	4	3	2	3	3	4	3	3	2	1	3	3	2	3	3		
Burwood Road	E	Between Parramatta Rd and Burton St	NP 630AM-930AM, 330PM-630PM MON-FRI, 1/2P 930AM-330PM, MON-FRI, 830AM-	3	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1	0	
Burton St	N	Between Broughton St and Lansdowne St	Unrestricted	4	10	8	8	8	9	10	10	10	9	8	7	6	7	7	9	6	8	7	8	8	8	7	7	7	8	8	8	8	8	
Burton St	S	Between Broughton St and Burwood Rd	Unrestricted	5	19	13	15	15	15	16	16	17	15	15	16	14	16	15	15	15	16	16	16	16	16	15	16	17	17	17	16	16	16	16
Burton St	N	Between Lansdowne St and Burwood Rd	Unrestricted	6	9	3	3	5	5	5	5	6	6	6	6	5	5	4	4	6	5	4	4	4	4	5	5	5	5	5	5	5	5	
Burton St	N	Between Burwood Rd and Loftus St	Unrestricted	7	20	18	17	17	18	18	18	18	17	17	18	17	17	18	18	17	18	15	13	13	13	13	12	12	12	14	14	14	14	14
Burton St	S	Between Burwood Rd and Loftus St	Unrestricted	8	21	13	17	17	17	17	17	17	17	17	17	17	16	17	17	16	15	15	15	13	14	16	12	8	9	14	14	13	14	14
Loftus St	E	Between Parramatta Rd and Gipps St (North of Works)	Unrestricted	9	11	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	8	7	6	5	5	4	4	4	4	4	4	4	4	4
Loftus St	W	Between Parramatta Rd and Burton St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	10	1	0	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	1	1	1	1	1	1	1	1	1	1	1
Loftus St	W	Between Parramatta Rd and Burton St	Unrestricted	11	7	9	9	9	9	9	9	9	9	9	9	9	9	9	9	8	8	8	8	5	4	2	2	2	2	2	2	2	2	2
Loftus St	E	Between Parramatta Rd and Gipps St (South of Works)	Unrestricted	12	5	2	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	1	2	2	2	2	2	2	2	2	2	2
Loftus St	W	Between Burton St and Gipps St	Unrestricted	13	7	6	6	6	6	6	6	6	6	6	5	6	6	6	6	6	6	5	4	3	4	4	4	5	5	5	5	5	5	5
Gipps St	N	Between Burwood Rd and Loftus St	Unrestricted	14	16	3	5	9	10	9	9	9	11	10	12	10	11	12	10	9	9	12	9	7	6	5	5	4	6	4	3	3	2	3
Gipps St	S	Between Burwood Rd and Loftus St	Unrestricted	15	13	8	9	12	11	10	10	11	11	11	14	12	11	11	11	11	11	12	12	12	12	12	11	8	9	9	9	8	8	8
Lansdowne St	W	Between Burton St and Gipps St	Unrestricted	16	20	5	5	5	6	3	3	3	3	4	3	3	3	3	3	4	4	5	5	3	3	4	4	5	5	7	7	4	4	4
Lansdowne St	E	Between Burton St and Gipps St	Unrestricted	17	23	5	4	6	6	5	7	6	6	7	6	7	7	7	7	7	7	6	5	7	7	6	6	5	5	4	3	3	3	3
Total occupied					104	114	125	128	124	126	128	126	129	133	129	131	128	133	129	127	126	118	108	102	100	98	87	90	105	102	96	97	96	
Supply					201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201

Saturday 11th September 2021				Date		11/09/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																													
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Burwood Road	W	Between Milton St and Parramatta Rd	NS 630AM-930AM, 330PM-700PM MON-FRI, 2P 930AM-330PM MON-FRI, 8AM-1PM	1	8	1	0	0	0	0	0	0	0	1	1	2	2	3	3	5	2	2	3	5	3	2	3	3	1	2	5	6	3	2	
Burwood Road	E	Between Milton St and Parramatta Rd	NS 630AM-930AM, 330PM-700PM MON-FRI, 2P 930AM-330PM MON-FRI, 8AM-1PM	2	5	2	1	1	1	1	2	1	1	1	2	2	3	1	1	3	2	1	2	3	2	2	3	1	2	3	3	3	3		
Burwood Road	E	Between Parramatta Rd and Burton St	NP 630AM-930AM, 330PM-630PM MON-FRI, 1/2P 930AM-330PM, MON-FRI, 830AM	3	6	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Burton St	N	Between Broughton St and Lansdowne St	Unrestricted	4	10	9	9	9	8	8	8	8	8	7	6	6	5	6	6	7	8	9	7	6	8	8	8	9	8	7	7	7	7	7	
Burton St	S	Between Broughton St and Burwood Rd	Unrestricted	5	19	14	14	14	14	14	14	13	15	14	15	15	13	14	14	11	13	13	14	13	14	14	15	14	13	15	15	14	14	14	
Burton St	N	Between Lansdowne St and Burwood Rd	Unrestricted	6	9	4	4	4	4	3	6	4	6	6	5	5	5	5	5	6	5	5	5	6	5	6	6	5	5	5	5	5	5	5	
Burton St	N	Between Burwood Rd and Loftus St	Unrestricted	7	20	15	15	17	15	14	14	14	14	14	11	11	11	11	10	11	13	13	13	15	14	14	14	11	11	12	13	13	14	14	
Burton St	S	Between Burwood Rd and Loftus St	Unrestricted	8	21	14	14	14	12	11	11	11	11	11	12	12	12	13	12	12	13	13	15	13	14	14	14	14	13	14	14	14	14	15	
Loftus St	E	Between Parramatta Rd and Gipps St (North of Works)	Unrestricted	9	11	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	5	4	4	4	4	4	4	4	4	4	4	4	4	4	
Loftus St	W	Between Parramatta Rd and Burton St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	10	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	
Loftus St	W	Between Parramatta Rd and Burton St	Unrestricted	11	7	4	4	4	4	4	5	5	6	6	6	6	5	5	5	5	5	5	4	4	6	6	6	6	6	6	6	6	6	6	
Loftus St	E	Between Parramatta Rd and Gipps St (South of Works)	Unrestricted	12	5	1	1	2	2	3	3	3	3	3	4	4	5	4	4	4	5	5	5	5	4	4	4	3	2	3	3	3	3	3	
Loftus St	W	Between Burton St and Gipps St	Unrestricted	13	7	3	2	2	2	2	2	2	3	2	3	2	2	2	1	2	3	3	3	4	4	4	4	4	4	4	4	4	4	4	4

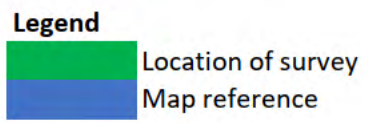
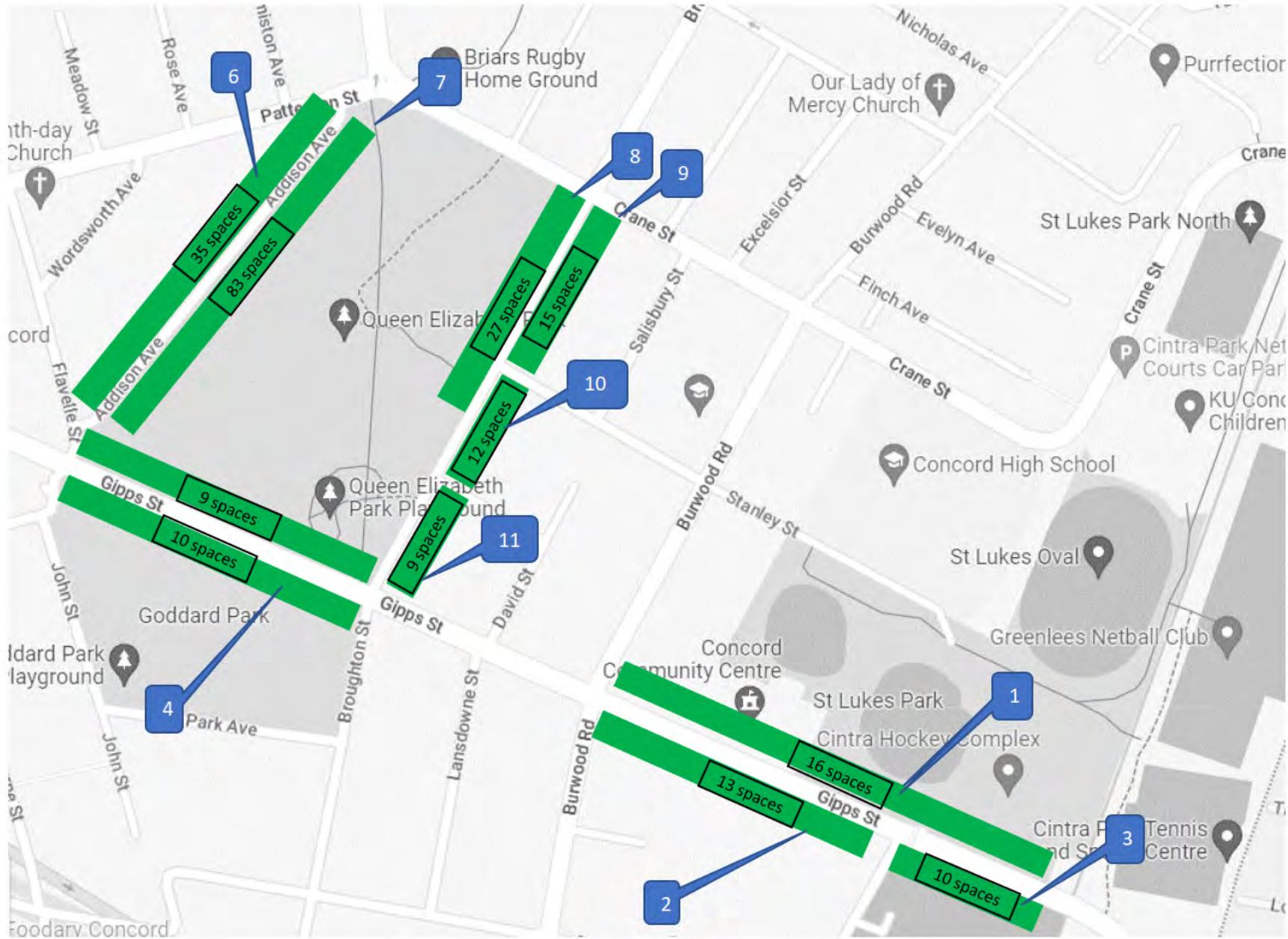
Gipps St	N	Between Burwood Rd and Loftus St	Unrestricted	14	16	3	2	3	5	3	5	5	5	5	5	6	5	5	3	7	4	5	5	5	5	4	3	3	3	4	3	4	3	3
Gipps St	S	Between Burwood Rd and Loftus St	Unrestricted	15	13	8	7	7	6	7	7	7	7	7	7	9	10	8	8	6	12	12	10	11	9	9	8	9	9	9	8	7	7	7
Lansdowne St	W	Between Burton St and Gipps St	Unrestricted	16	20	4	4	5	5	5	5	5	5	7	5	4	5	6	5	5	6	6	5	5	5	5	5	5	5	5	5	5	5	5
Lansdowne St	E	Between Burton St and Gipps St	Unrestricted	17	23	7	7	7	7	7	5	5	5	6	4	4	5	6	5	5	4	4	5	5	5	3	4	6	6	5	5	5	5	5
Total occupied					93	88	93	89	86	91	87	93	94	90	93	93	93	87	93	101	102	101	104	104	99	102	99	95	98	101	101	98	98	
Supply					201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201

Sunday 12th September 2021					Date	12/09/2021																												
					Time																													
Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Burwood Road	W	Between Milton St and Parramatta Rd	NS 630AM-930AM, 330PM-700PM MON-FRI, 2P 930AM-330PM MON-FRI, 8AM-1PM	1	8	0	0	0	0	0	0	0	0	1	3	4	7	7	3	6	6	5	4	4	5	4	5	5	6	5	5	5	2	
Burwood Road	E	Between Milton St and Parramatta Rd	NS 630AM-930AM, 330PM-700PM MON-FRI, 2P 930AM-330PM MON-FRI, 8AM-1PM	2	5	3	3	3	3	3	3	3	3	3	2	2	3	5	5	5	4	3	2	3	2	2	2	3	3	4	4	1		
Burwood Road	E	Between Parramatta Rd and Burton St	NP 630AM-930AM, 330PM-630PM MON-FRI, 1/2P 930AM-330PM, MON-FRI, 830AM-	3	6	1	1	1	1	1	1	1	1	1	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Burton St	N	Between Broughton St and Lansdowne St	Unrestricted	4	10	9	9	9	9	9	9	8	8	8	8	8	7	7	8	9	9	9	9	9	9	9	8	8	8	9	9	9	9	
Burton St	S	Between Broughton St and Burwood Rd	Unrestricted	5	19	15	15	14	14	14	14	14	12	13	11	12	13	11	12	12	12	12	12	10	11	11	10	11	11	11	11	11	11	11
Burton St	N	Between Lansdowne St and Burwood Rd	Unrestricted	6	9	5	5	5	5	5	4	3	3	3	4	4	4	4	3	3	3	4	5	5	5	5	5	6	5	5	5	5	5	
Burton St	N	Between Burwood Rd and Loftus St	Unrestricted	7	20	12	12	13	13	13	14	14	13	13	14	16	18	17	17	17	15	15	14	13	13	16	16	16	16	16	16	16	16	16
Burton St	S	Between Burwood Rd and Loftus St	Unrestricted	8	21	11	11	11	11	12	12	12	9	9	11	10	11	11	10	10	10	10	11	10	11	11	14	15	13	14	14	14	14	14
Loftus St	E	Between Parramatta Rd and Gipps St (North of Works)	Unrestricted	9	11	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	5	4	4	4	4	4	4	4	4	
Loftus St	W	Between Parramatta Rd and Burton St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	10	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1	1	1	1	1	1	
Loftus St	W	Between Parramatta Rd and Burton St	Unrestricted	11	7	3	3	3	3	4	3	3	2	1	1	1	1	1	1	1	1	1	1	1	1	1	2	2	2	2	2	2	2	
Loftus St	E	Between Parramatta Rd and Gipps St (South of Works)	Unrestricted	12	5	3	3	3	3	3	3	3	3	2	3	3	3	3	3	3	3	3	3	3	3	1	0	0	0	0	0	0	0	
Loftus St	W	Between Burton St and Gipps St	Unrestricted	13	7	4	4	4	4	4	4	4	4	4	3	3	3	3	4	5	6	5	6	5	5	4	4	4	4	4	4	4	4	
Gipps St	N	Between Burwood Rd and Loftus St	Unrestricted	14	16	3	4	3	5	4	4	3	4	6	5	5	6	6	5	5	4	7	6	5	5	6	6	5	5	7	7	5	5	
Gipps St	S	Between Burwood Rd and Loftus St	Unrestricted	15	13	7	6	6	6	6	6	7	8	8	9	8	6	5	5	6	5	6	5	5	7	6	6	6	9	8	8	9	8	
Lansdowne St	W	Between Burton St and Gipps St	Unrestricted	16	20	5	5	5	5	5	5	5	5	4	4	5	4	3	3	4	4	4	4	3	4	4	3	4	4	5	4	4	4	
Lansdowne St	E	Between Burton St and Gipps St	Unrestricted	17	23	6	6	6	7	7	5	5	5	5	5	5	5	6	5	5	5	7	7	6	6	6	5	6	5	4	3	2	2	
Total occupied					91	91	90	93	93	93	90	85	86	89	92	92	91	92	91	91	97	91	87	92	92	91	94	96	100	97	96	94	87	
Supply					201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	

Monday 13th September 2021					Date	13/09/2021																												
					Time																													
Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Burwood Road	W	Between Milton St and Parramatta Rd	NS 630AM-930AM, 330PM-700PM MON-FRI, 2P 930AM-330PM MON-FRI, 8AM-1PM	1	8	1	0	1	2	1	0	1	0	3	4	6	3	7	6	5	2	3	5	5	2	5	4	0	4	4	2	6	6	3
Burwood Road	E	Between Milton St and Parramatta Rd	NS 630AM-930AM, 330PM-700PM MON-FRI, 2P 930AM-330PM MON-FRI, 8AM-1PM	2	5	1	1	1	1	1	1	1	1	2	3	1	3	3	3	2	3	2	3	2	3	2	2	3	1	2	2	3	3	
Burwood Road	E	Between Parramatta Rd and Burton St	NP 630AM-930AM, 330PM-630PM MON-FRI, 1/2P 930AM-330PM, MON-FRI, 830AM-	3	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	3	3	0	0	0	0	0	0	0	0	0	0	0
Burton St	N	Between Broughton St and Lansdowne St	Unrestricted	4	10	10	10	10	10	10	10	10	10	10	9	9	10	10	10	10	10	10	10	10	10	8	8	7	7	8	8	8	8	
Burton St	S	Between Broughton St and Burwood Rd	Unrestricted	5	19	11	11	13	15	15	14	14	15	15	15	14	14	16	16	16	16	16	16	14	13	12	14	14	14	15	15	15	15	
Burton St	N	Between Lansdowne St and Burwood Rd	Unrestricted	6	9	4	4	6	5	4	4	5	4	5	5	6	6	7	7	7	7	7	6	5	6	5	6	6	4	5	4	4	4	
Burton St	N	Between Burwood Rd and Loftus St	Unrestricted	7	20	14	14	15	16	15	14	15	15	14	17	16	16	16	14	14	15	16	15	17	14	14	14	14	14	14	15	15	15	15
Burton St	S	Between Burwood Rd and Loftus St	Unrestricted	8	21	14	14	14	14	15	14	14	14	13	14	9	12	11	11	9	12	9	12	13	13	13	13	13	12	12	12	12	12	12

Street	Side	Area	Restriction	Map reference	Supply	10	7	7	7	8	9	8	8	8	8	8	8	9	8	8	9	9	8	8	8	8	7	7	9	7	7	7	7	7				
Burton St	N	Between Broughton St and Lansdowne St	Unrestricted	4	10	7	7	7	8	9	8	8	8	8	8	8	8	9	8	8	9	9	8	8	8	8	7	7	9	7	7	7	7	7				
Burton St	S	Between Broughton St and Burwood Rd	Unrestricted	5	19	15	14	12	14	13	12	13	13	15	13	15	13	12	13	14	12	13	13	13	13	13	14	15	13	14	15	15	15	15	15			
Burton St	N	Between Lansdowne St and Burwood Rd	Unrestricted	6	9	3	4	6	6	6	7	7	7	7	6	8	7	7	7	6	7	6	7	6	6	7	5	5	4	5	5	5	5	5	5			
Burton St	N	Between Burwood Rd and Loftus St	Unrestricted	7	20	19	18	17	17	16	17	18	15	16	17	16	17	16	17	17	17	15	16	14	16	13	14	15	13	13	13	14	14	13	13	13		
Burton St	S	Between Burwood Rd and Loftus St	Unrestricted	8	21	17	17	17	17	16	16	15	16	16	17	17	16	16	16	16	18	18	17	18	17	17	15	15	15	15	15	15	15	15	15	15	15	
Loftus St	E	Between Parramatta Rd and Gipps St (North of Works)	Unrestricted	9	11	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	11	10	9	8	8	6	3	3	4	3	3	3	3	3		
Loftus St	W	Between Parramatta Rd and Burton St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	10	1	1	1	2	2	2	2	2	2	2	1	1	1	1	1	1	1	1	1	1	1	1	1	0	0	0	0	0	0	0	0	0		
Loftus St	W	Between Parramatta Rd and Burton St	Unrestricted	11	7	8	8	8	7	7	7	7	7	7	6	7	7	7	7	7	7	6	6	6	6	4	3	2	2	2	2	3	4	4	4	4		
Loftus St	E	Between Parramatta Rd and Gipps St (South of Works)	Unrestricted	12	5	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	3	5	2	2	2	2	2	2	2	2	2		
Loftus St	W	Between Burton St and Gipps St	Unrestricted	13	7	4	7	7	7	7	7	7	7	7	7	7	7	5	5	5	5	6	5	5	4	4	4	3	4	4	4	4	4	4	4	4	4	
Gipps St	N	Between Burwood Rd and Loftus St	Unrestricted	14	16	6	6	9	9	9	8	9	9	9	6	7	8	8	7	7	7	7	7	6	3	4	4	6	5	5	7	5	6	6	6	6	6	
Gipps St	S	Between Burwood Rd and Loftus St	Unrestricted	15	13	13	13	11	9	10	11	11	11	10	10	9	9	9	9	9	9	8	8	11	11	12	9	12	12	11	11	11	11	11	9	9	9	9
Lansdowne St	W	Between Burton St and Gipps St	Unrestricted	16	20	4	4	4	6	5	5	7	7	6	6	6	6	6	6	5	5	5	5	4	4	4	4	4	3	3	3	3	2	2	2	2	2	
Lansdowne St	E	Between Burton St and Gipps St	Unrestricted	17	23	4	4	4	5	5	5	4	4	6	5	6	7	6	6	6	6	6	5	6	4	3	2	2	3	2	2	2	2	2	2	2	2	
Total occupied					117	118	119	123	120	120	125	122	125	124	130	130	124	121	123	119	121	122	116	106	100	98	93	93	95	100	97	91	91	91	91	91	91	
Supply					201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	

Thursday 16th September 2021					Date	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021	16/09/2021		
Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM				
Burwood Road	W	Between Milton St and Parramatta Rd	NS 630AM-930AM, 330PM-700PM MON-FRI, 2P 930AM-330PM MON-FRI, 8AM-1PM	1	8	0	0	0	0	0	0	0	2	2	3	3	4	3	5	7	3	5	5	6	5	2	3	3	5	7	8	5	4	3	3			
Burwood Road	E	Between Milton St and Parramatta Rd	NS 630AM-930AM, 330PM-700PM MON-FRI, 2P 930AM-330PM MON-FRI, 8AM-1PM	2	5	1	1	1	1	1	1	1	2	3	3	3	2	3	3	3	3	3	3	3	2	2	1	3	2	3	3	2	2	2	2	2		
Burwood Road	E	Between Parramatta Rd and Burton St	NP 630AM-930AM, 330PM-630PM MON-FRI, 1/2P 930AM-330PM, MON-FRI, 830AM	3	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Burton St	N	Between Broughton St and Lansdowne St	Unrestricted	4	10	9	9	9	10	10	10	10	10	9	8	8	8	7	8	9	8	8	9	8	7	8	8	7	7	6	6	6	6	6	6	6	6	
Burton St	S	Between Broughton St and Burwood Rd	Unrestricted	5	19	10	10	13	15	16	16	16	15	13	12	11	14	11	10	8	8	12	13	14	14	12	11	10	12	14	14	14	14	14	14	14	14	
Burton St	N	Between Lansdowne St and Burwood Rd	Unrestricted	6	9	5	5	5	6	6	6	6	6	6	6	5	5	5	6	4	3	4	4	4	4	4	4	4	5	3	3	3	3	3	3	3		
Burton St	N	Between Burwood Rd and Loftus St	Unrestricted	7	20	16	18	19	19	17	19	19	19	19	18	19	19	19	19	19	19	19	20	19	18	12	13	13	12	11	11	11	11	11	11	11	11	
Burton St	S	Between Burwood Rd and Loftus St	Unrestricted	8	21	13	16	16	16	16	16	16	16	16	16	16	16	16	15	16	16	16	15	17	15	14	14	13	13	10	10	11	11	11	11	11	11	
Loftus St	E	Between Parramatta Rd and Gipps St (North of Works)	Unrestricted	9	11	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	10	9	9	9	6	6	5	5	5	5	5	5	5	5	5	
Loftus St	W	Between Parramatta Rd and Burton St	1/2P 830AM-6PM MON-FRI, 830AM-1230PM SAT	10	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	
Loftus St	W	Between Parramatta Rd and Burton St	Unrestricted	11	7	7	8	8	8	8	8	8	8	8	8	8	8	8	8	8	8	8	8	8	8	4	5	5	5	5	5	5	5	5	5	5	5	
Loftus St	E	Between Parramatta Rd and Gipps St (South of Works)	Unrestricted	12	5	4	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	5	4	3	2	3	2	2	2	2	2	2	2	2		
Loftus St	W	Between Burton St and Gipps St	Unrestricted	13	7	7	7	7	7	7	7	7	7	7	7	7	7	7	6	7	7	7	7	6	4	5	6	6	4	4	4	4	4	4	4	4	4	
Gipps St	N	Between Burwood Rd and Loftus St	Unrestricted	14	16	8	7	6	5	8	7	6	7	6	8	9	8	7	11	9	9	10	12	8	9	9	8	9	9	8	7	7	7	7	7	7	7	
Gipps St	S	Between Burwood Rd and Loftus St	Unrestricted	15	13	15	13	10	10	10	10	10	10	11	10	9	10	10	13	12	11	11	10	11	8	8	8	10	14	13	12	12	11	11	11	11	11	11
Lansdowne St	W	Between Burton St and Gipps St	Unrestricted	16	20	3	3	4	4	4	5	5	5	5	5	3	3	3	4	3	3	3	3	3	3	5	6	5	5	5	5	5	5	5	5	5	5	
Lansdowne St	E	Between Burton St and Gipps St	Unrestricted	17	23	6	5	5	5	5	6	6	5	5	5	5	5	5	5	5	6	6	5	5	6	6	7	6	4	4	3	3	3	3	3	3		
Total occupied					115	118	119	122	124	127	126	128	126	125	122	125	119	130	127	120	127	131	125	105	106	100	100	100	99	99	95	93	92	92	92	92	92	
Supply					201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	201	



Monday 6th February 2023				Date		6/02/2023	6/02/2023	6/02/2023	6/02/2023	6/02/2023	6/02/2023	6/02/2023	6/02/2023	6/02/2023	6/02/2023	6/02/2023	6/02/2023	6/02/2023	6/02/2023	6/02/2023
				Time																
Street	Side	Area	Restriction	Map reference	Supply	6AM	7AM	8AM	9AM	10AM	11AM	12PM	1PM	2PM	3PM	4PM	5PM	6PM	7PM	8PM
Broughton St	W	Gipps St To Crane St	Disabled Parking	8	6	0	1	0	0	0	4	2	0	0	0	0	0	0	0	0
Broughton St	W	Gipps St To Crane St	Unrestricted	8	27	9	7	8	13	11	13	10	10	11	11	15	14	12	11	10
Broughton St	E	Gipps St to David St	Unrestricted	11	9	4	4	4	4	5	3	3	3	3	4	3	5	6	5	5
Broughton St	E	David St to Stanley St	Unrestricted	10	12	6	6	3	8	2	3	5	5	4	7	7	6	7	5	5
Broughton St	E	Stanley St to Crane St	Unrestricted	9	15	9	9	7	8	9	4	6	7	6	6	6	5	7	8	6
Addison Ave	E	Patterson St To Flavelle St	Unrestricted	7	83	12	11	13	13	14	11	12	11	9	10	14	20	16	10	9
Addison Ave	E	Patterson St To Flavelle St	Disabled Parking	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Addison Ave	W	Patterson St To Flavelle St	Unrestricted	6	35	5	5	4	5	6	5	5	3	5	5	6	6	5	3	3
Gipps St	N	Flavelle St to Broughton St	Unrestricted	5	9	1	2	4	2	2	2	1	1	2	1	1	2	2	2	2
Gipps St	S	Flavelle St to Broughton St	Unrestricted	4	10	0	1	1	0	0	1	0	0	0	0	0	1	0	0	0
Gipps St	N	Burwood Rd to Cintra Hockey Complex	Unrestricted	1	16	2	2	2	3	2	3	4	3	2	3	2	3	6	6	5
Gipps St	S	Burwood Rd to Loftus St	Unrestricted	2	13	9	8	9	8	6	6	6	8	6	7	8	8	10	13	11
Gipps St	S	Loftus St to Cintra Hockey Complex	Unrestricted	3	10	1	1	1	1	1	2	2	1	1	1	1	1	1	1	1
Total occupied						58	57	56	65	58	57	56	52	49	55	63	71	72	64	57
Supply						246	246	246	246	246	246	246	246	246	246	246	246	246	246	246

Tuesday 7th February 2023				Date		7/02/2023	7/02/2023	7/02/2023	7/02/2023	7/02/2023	7/02/2023	7/02/2023	7/02/2023	7/02/2023	7/02/2023	7/02/2023	7/02/2023	7/02/2023	7/02/2023	7/02/2023
				Time																
Street	Side	Area	Restriction	Map reference	Supply	6AM	7AM	8AM	9AM	10AM	11AM	12PM	1PM	2PM	3PM	4PM	5PM	6PM	7PM	8PM
Broughton St	W	Gipps St To Crane St	Disabled Parking	8	6	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0
Broughton St	W	Gipps St To Crane St	Unrestricted	8	27	8	8	8	16	17	12	8	10	13	24	16	14	14	13	11
Broughton St	E	Gipps St to David St	Unrestricted	11	9	4	5	4	3	2	2	2	2	3	3	3	3	3	5	4
Broughton St	E	David St to Stanley St	Unrestricted	10	12	6	6	4	8	7	8	7	6	6	11	7	8	5	7	7

Broughton St	E	Stanley St to Crane St	Unrestricted	9	15	7	6	6	11	8	8	7	6	7	11	8	7	10	10	11
Addison Ave	E	Patterson St To Flavelle St	Unrestricted	7	83	10	13	11	16	12	14	13	14	13	16	12	11	27	29	32
Addison Ave	E	Patterson St To Flavelle St	Disabled Parking	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Addison Ave	W	Patterson St To Flavelle St	Unrestricted	6	35	5	3	3	3	2	3	2	0	3	3	4	5	7	7	8
Gipps St	N	Flavelle St to Broughton St	Unrestricted	5	9	2	4	2	3	2	2	1	2	2	1	1	1	1	2	1
Gipps St	S	Flavelle St to Broughton St	Unrestricted	4	10	0	0	0	0	0	0	0	0	0	1	0	0	8	5	0
Gipps St	N	Burwood Rd to Cintra Hockey Complex	Unrestricted	1	16	4	4	6	10	12	12	12	11	12	11	10	11	12	8	6
Gipps St	S	Burwood Rd to Loftus St	Unrestricted	2	13	10	11	9	11	12	12	8	6	8	5	8	13	18	19	19
Gipps St	S	Loftus St to Cintra Hockey Complex	Unrestricted	3	10	2	2	4	3	3	3	4	1	2	3	2	2	1	1	1
Total occupied					58	62	57	84	77	76	65	58	69	89	71	75	106	106	100	
Supply					246	246	246	246	246	246	246	246	246	246	246	246	246	246	246	

Wednesday 8th February 2023					Date	8/02/2023	8/02/2023	8/02/2023	8/02/2023	8/02/2023	8/02/2023	8/02/2023	8/02/2023	8/02/2023	8/02/2023	8/02/2023	8/02/2023	8/02/2023	8/02/2023	8/02/2023
					Time	6AM	7AM	8AM	9AM	10AM	11AM	12PM	1PM	2PM	3PM	4PM	5PM	6PM	7PM	8PM
Street	Side	Area	Restriction	Map reference	Supply															
Broughton St	W	Gipps St To Crane St	Disabled Parking	8	6	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0
Broughton St	W	Gipps St To Crane St	Unrestricted	8	27	8	7	8	11	16	14	12	10	13	21	15	19	12	9	9
Broughton St	E	Gipps St to David St	Unrestricted	11	9	4	4	5	2	3	3	4	4	3	2	3	3	3	4	5
Broughton St	E	David St to Stanley St	Unrestricted	10	12	5	5	5	4	4	4	5	6	5	10	3	3	5	5	6
Broughton St	E	Stanley St to Crane St	Unrestricted	9	15	8	7	5	8	7	7	6	8	7	11	10	10	9	11	10
Addison Ave	E	Patterson St To Flavelle St	Unrestricted	7	83	13	15	13	14	9	10	10	9	9	10	11	41	53	16	10
Addison Ave	E	Patterson St To Flavelle St	Disabled Parking	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Addison Ave	W	Patterson St To Flavelle St	Unrestricted	6	35	5	5	4	3	5	5	4	5	4	3	3	4	9	4	4
Gipps St	N	Flavelle St to Broughton St	Unrestricted	5	9	1	2	3	2	2	1	1	2	4	2	1	1	1	1	1

Gipps St	S	Flavelle St to Broughton St	Unrestricted	4	10	0	0	1	0	0	0	0	0	0	2	0	0	0	0	0
Gipps St	N	Burwood Rd to Cintra Hockey Complex	Unrestricted	1	16	2	1	2	3	2	3	3	3	2	3	2	3	6	6	5
Gipps St	S	Burwood Rd to Loftus St	Unrestricted	2	13	9	9	9	8	7	6	6	8	6	6	8	7	10	11	11
Gipps St	S	Loftus St to Cintra Hockey Complex	Unrestricted	3	10	1	1	1	1	1	2	2	1	1	1	1	0	0	2	4
Total occupied						56	56	56	56	56	55	53	56	54	72	57	91	108	69	65
Supply						246	246	246	246	246	246	246	246	246	246	246	246	246	246	246

Thursday 9th February 2023					Date	9/02/2023	9/02/2023	9/02/2023	9/02/2023	9/02/2023	9/02/2023	9/02/2023	9/02/2023	9/02/2023	9/02/2023	9/02/2023	9/02/2023	9/02/2023	9/02/2023	9/02/2023				
Street	Side	Area	Restriction	Map reference	Time																			
					Supply	6AM	7AM	8AM	9AM	10AM	11AM	12PM	1PM	2PM	3PM	4PM	5PM	6PM	7PM	8PM				
Broughton St	W	Gipps St To Crane St	Disabled Parking	8	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0				
Broughton St	W	Gipps St To Crane St	Unrestricted	8	27	8	9	8	16	12	12	12	10	10	19	11	10	11	11	13				
Broughton St	E	Gipps St to David St	Unrestricted	11	9	5	4	4	3	6	4	4	3	2	3	2	3	6	6	4				
Broughton St	E	David St to Stanley St	Unrestricted	10	12	6	5	4	8	7	8	7	6	4	10	4	4	6	6	6				
Broughton St	E	Stanley St to Crane St	Unrestricted	9	15	7	8	7	8	5	5	5	7	11	12	11	8	11	11	12				
Addison Ave	E	Patterson St To Flavelle St	Unrestricted	7	83	9	9	10	9	9	10	10	10	9	10	12	9	10	10	12				
Addison Ave	E	Patterson St To Flavelle St	Disabled Parking	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0				
Addison Ave	W	Patterson St To Flavelle St	Unrestricted	6	35	6	5	4	3	3	4	5	6	4	2	4	5	4	4	2				
Gipps St	N	Flavelle St to Broughton St	Unrestricted	5	9	1	2	1	1	1	1	1	1	3	3	2	2	1	1	1				
Gipps St	S	Flavelle St to Broughton St	Unrestricted	4	10	0	0	0	0	0	0	0	2	0	3	0	1	1	1	1				
Gipps St	N	Burwood Rd to Cintra Hockey Complex	Unrestricted	1	16	2	4	4	7	6	5	6	6	8	7	4	5	10	10	7				
Gipps St	S	Burwood Rd to Loftus St	Unrestricted	2	13	9	9	8	7	8	7	8	5	7	6	8	9	16	16	16				
Gipps St	S	Loftus St to Cintra Hockey Complex	Unrestricted	3	10	1	1	1	1	3	1	1	2	3	2	0	0	0	0	0				
Total occupied						54	56	51	63	60	57	59	58	61	77	58	56	76	76	74				
Supply						246	246	246	246	246	246	246	246	246	246	246	246	246	246	246				

Friday 10th February 2023					Date	10/02/2023	10/02/2023	10/02/2023	10/02/2023	10/02/2023	10/02/2023	10/02/2023	10/02/2023	10/02/2023	10/02/2023	10/02/2023	10/02/2023	10/02/2023	10/02/2023	10/02/2023				
					Time																			

Street	Side	Area	Restriction	Map reference	Supply	6AM	7AM	8AM	9AM	10AM	11AM	12PM	1PM	2PM	3PM	4PM	5PM	6PM	7PM	8PM
Broughton St	W	Gipps St To Crane St	Disabled Parking	8	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Broughton St	W	Gipps St To Crane St	Unrestricted	8	27	10	10	8	11	9	10	11	9	10	19	14	18	15	14	13
Broughton St	E	Gipps St to David St	Unrestricted	11	9	5	5	4	4	4	5	5	5	5	5	4	5	3	4	2
Broughton St	E	David St to Stanley St	Unrestricted	10	12	6	6	4	9	8	10	8	6	6	10	4	5	3	3	5
Broughton St	E	Stanley St to Crane St	Unrestricted	9	15	9	10	9	7	5	10	6	7	5	11	7	7	12	9	8
Addison Ave	E	Patterson St To Flavelle St	Unrestricted	7	83	11	10	12	12	15	14	12	10	10	14	12	11	12	12	13
Addison Ave	E	Patterson St To Flavelle St	Disabled Parking	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Addison Ave	W	Patterson St To Flavelle St	Unrestricted	6	35	3	2	2	2	3	1	3	3	3	2	3	4	3	3	3
Gipps St	N	Flavelle St to Broughton St	Unrestricted	5	9	1	1	1	1	2	3	1	1	2	1	1	1	1	1	1
Gipps St	S	Flavelle St to Broughton St	Unrestricted	4	10	0	0	1	0	0	2	0	0	0	1	0	1	1	1	1
Gipps St	N	Burwood Rd to Cintra Hockey Complex	Unrestricted	1	16	4	5	5	6	9	9	8	8	9	9	5	6	3	5	6
Gipps St	S	Burwood Rd to Loftus St	Unrestricted	2	13	9	9	10	9	10	12	11	7	9	8	8	9	11	12	12
Gipps St	S	Loftus St to Cintra Hockey Complex	Unrestricted	3	10	1	1	1	1	1	2	1	2	3	2	1	1	0	0	3
Total occupied						59	59	57	62	66	78	66	58	62	82	59	68	64	64	67
Supply						246	246	246	246	246	246	246	246	246	246	246	246	246	246	246

Saturday 11th February 2023					Date	11/02/2023	11/02/2023	11/02/2023	11/02/2023	11/02/2023	11/02/2023	11/02/2023	11/02/2023	11/02/2023	11/02/2023	11/02/2023	11/02/2023	11/02/2023	11/02/2023	11/02/2023
					Time															
Street	Side	Area	Restriction	Map reference	Supply	6AM	7AM	8AM	9AM	10AM	11AM	12PM	1PM	2PM	3PM	4PM	5PM	6PM	7PM	8PM
Broughton St	W	Gipps St To Crane St	Disabled Parking	8	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Broughton St	W	Gipps St To Crane St	Unrestricted	8	27	13	12	12	13	19	18	11	14	17	16	18	17	15	15	12
Broughton St	E	Gipps St to David St	Unrestricted	11	9	2	2	2	3	1	1	4	3	3	2	2	2	3	3	3
Broughton St	E	David St to Stanley St	Unrestricted	10	12	5	5	4	4	6	6	3	5	6	6	6	5	5	6	6
Broughton St	E	Stanley St to Crane St	Unrestricted	9	15	11	10	10	8	8	7	8	8	10	10	9	8	6	9	10

Addison Ave	E	Patterson St To Flavelle St	Unrestricted	7	83	10	10	10	14	28	31	31	13	25	26	25	24	28	25	11
Addison Ave	E	Patterson St To Flavelle St	Disabled Parking	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Addison Ave	W	Patterson St To Flavelle St	Unrestricted	6	35	6	6	4	3	3	3	4	6	7	4	5	6	6	4	2
Gipps St	N	Flavelle St to Broughton St	Unrestricted	5	9	1	1	1	4	9	6	2	2	2	3	2	2	1	1	1
Gipps St	S	Flavelle St to Broughton St	Unrestricted	4	10	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0
Gipps St	N	Burwood Rd to Cintra Hockey Complex	Unrestricted	1	16	3	3	5	13	12	6	4	2	1	3	5	5	3	5	6
Gipps St	S	Burwood Rd to Loftus St	Unrestricted	2	13	12	12	15	20	19	15	12	13	13	13	14	12	11	11	11
Gipps St	S	Loftus St to Cintra Hockey Complex	Unrestricted	3	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Total occupied					63	61	63	82	105	94	79	66	85	83	86	81	78	79	63	
Supply					246	246	246	246	246	246	246	246	246	246	246	246	246	246	246	246

Sunday 12th February 2023					Date	12/02/2023	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	13/10/2021	
Street	Side	Area	Restriction	Map reference	Time																
					Supply	6AM	7AM	8AM	9AM	10AM	11AM	12PM	1PM	2PM	3PM	4PM	5PM	6PM	7PM	8PM	
Broughton St	W	Gipps St To Crane St	Disabled Parking	8	6	0	0	0	0	1	1	1	0	0	0	0	0	0	1	1	
Broughton St	W	Gipps St To Crane St	Unrestricted	8	27	13	13	15	20	25	24	23	17	15	16	15	17	13	15	13	
Broughton St	E	Gipps St to David St	Unrestricted	11	9	4	4	5	5	5	5	4	4	4	5	4	4	4	6	6	
Broughton St	E	David St to Stanley St	Unrestricted	10	12	6	6	5	5	9	9	5	6	8	4	4	6	6	7	9	
Broughton St	E	Stanley St to Crane St	Unrestricted	9	15	10	9	9	10	11	10	8	10	9	9	7	7	8	7	8	
Addison Ave	E	Patterson St To Flavelle St	Unrestricted	7	83	12	12	15	27	29	26	21	19	21	20	18	19	15	16	11	
Addison Ave	E	Patterson St To Flavelle St	Disabled Parking	7	1	0	0	0	0	0	0	0	1	1	1	1	1	1	1	0	
Addison Ave	W	Patterson St To Flavelle St	Unrestricted	6	35	6	6	6	5	6	7	7	7	7	6	7	8	8	5	6	
Gipps St	N	Flavelle St to Broughton St	Unrestricted	5	9	0	0	0	5	2	1	1	2	2	0	0	0	0	0	0	
Gipps St	S	Flavelle St to Broughton St	Unrestricted	4	10	0	0	4	6	0	0	0	0	0	0	0	1	2	1	0	

Gipps St	N	Burwood Rd to Cintra Hockey Complex	Unrestricted	1	16	6	6	8	8	7	7	7	7	5	6	6	6	6	7	4	
Gipps St	S	Burwood Rd to Loftus St	Unrestricted	2	13	12	12	11	11	11	10	9	11	11	11	11	11	11	12	12	12
Gipps St	S	Loftus St to Cintra Hockey Complex	Unrestricted	3	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total occupied					69	68	78	102	106	100	86	84	83	78	73	80	75	78	70		
Supply					246	246	246	246	246	246	246	246	246	246	246	246	246	246	246	246	



Legend
Green bar: Location of survey
Blue bar: Map reference

Wednesday 13th October 2021				Date		13/10/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																													
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Queen St	W	Between Shipley Ave and Sydney Trains Driveway	Unrestricted	1	3	2	2	2	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	2	1	1	1	1	0	
Queen St	E	Between Shipley Ave and Wellbank St	Unrestricted	2	13	9	9	9	11	11	11	11	11	11	11	11	9	11	9	9	10	9	10	8	9	10	9	9	9	8	9	5	6		
Queen St	E	Between Wellbank St and Waratah St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	9	9	11	10	10	10	8	8	10	9	8	9	10	12	10	9	13	9	15	12	8	9	9	5	8	10	11	8	4	4	
Queen St	W	Between Train Station Service Driveway and Power Pole	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN	4	5	1	1	1	5	4	2	6	4	5	5	5	6	4	5	4	3	5	6	4	4	3	3	3	4	4	4	1	2		
Queen St	W	Between Bus Stop and Train Station Service Driveway	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	5	2	3	3	1	2	3	4	3	2	4	4	6	4	3	4	2	4	5	3	4	4	3	1	4	4	2	5	4	3		
Queen St	W	Between Sydney Trains Driveway and Wellbank St	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	6	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	0	0	0	3	0	0	0	
Queen St	W	Between Sydney Trains Driveway and Wellbank St	Disabled	7	1	0	0	1	0	0	1	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	1	1	1	1	0	0	
Queen St	W	Between Sydney Trains Driveway and Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	8	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1	0	0	0	0	0	
Queen St	W	North of Power Pole	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	9	4	1	1	2	2	3	4	4	4	5	5	4	4	4	5	4	5	5	5	4	6	6	4	4	2	6	4	1	1	1	
Queen St	E	North of Waratah St	Unrestricted	10	9	3	4	4	1	2	3	2	3	3	4	4	4	4	6	6	6	5	6	5	5	5	5	5	5	6	5	5	5	5	
Wellbank St	N	Between Queen St and 115 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	11	3	1	2	2	1	1	3	3	4	3	3	2	2	2	3	2	2	2	3	1	2	1	2	3	3	1	0	0	0	0	
Wellbank St	S	Between Queen St and 158 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	12	1	0	1	0	0	0	1	1	1	1	1	0	0	0	1	1	0	0	0	0	0	0	1	0	0	1	2	1	0	0	0
Total occupied					29	34	32	35	38	40	42	42	45	45	44	44	41	48	40	45	44	50	44	42	43	39	37	38	44	40	33	21	21		
Supply					53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	

Thursday 14th October 2021				Date		14/10/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																													
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Queen St	W	Between Shipley Ave and Sydney Trains Driveway	Unrestricted	1	3	2	3	2	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	2	2	2	2	0	0		
Queen St	E	Between Shipley Ave and Wellbank St	Unrestricted	2	13	10	8	8	9	10	11	11	12	9	8	9	11	11	9	9	10	10	9	9	10	10	9	8	8	8	5	6	3	3	
Queen St	E	Between Wellbank St and Waratah St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	9	6	10	12	11	9	7	7	7	10	12	7	10	12	10	9	9	9	9	8	7	9	7	6	5	6	5	8	4	4	
Queen St	W	Between Train Station Service Driveway and Power Pole	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN	4	5	0	0	1	4	2	3	5	2	3	3	3	3	4	3	4	4	4	5	2	1	3	4	3	3	3	4	0	0		
Queen St	W	Between Bus Stop and Train Station Service Driveway	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	5	2	1	4	3	3	6	2	4	3	3	4	2	4	4	5	3	2	2	3	3	3	4	2	4	5	5	4	5	1	1	
Queen St	W	Between Sydney Trains Driveway and Wellbank St	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	6	1	0	1	1	1	1	0	0	0	1	1	1	0	0	0	0	1	0	1	0	0	0	0	1	1	0	0	1	2	0	
Queen St	W	Between Sydney Trains Driveway and Wellbank St	Disabled	7	1	0	0	0	0	0	1	1	1	1	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	0	
Queen St	W	Between Sydney Trains Driveway and Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	8	2	0	0	0	0	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	1	0	0	1	0	0	
Queen St	W	North of Power Pole	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	9	4	1	2	3	4	4	2	4	4	3	5	4	3	4	4	4	6	5	5	3	1	2	4	3	3	4	5	5	3	2	
Queen St	E	North of Waratah St	Unrestricted	10	9	1	1	3	3	3	3	3	3	3	3	4	4	3	3	4	5	3	3	4	4	5	4	3	3	4	3	3	4	5	4
Wellbank St	N	Between Queen St and 115 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	11	3	1	1	2	3	2	2	1	2	1	1	1	1	1	1	1	1	1	1	2	2	1	2	1	1	1	1	1	1	1	
Wellbank St	S	Between Queen St and 158 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	12	1	1	1	0	0	0	1	1	2	0	1	1	2	1	1	1	1	1	1	0	1	0	1	1	0	0	1	0	0	0	0
Total occupied					23	31	35	41	41	36	41	40	38	44	37	41	45	41	41	40	40	41	35	34	37	35	33	32	32	28	38	21	15		
Supply					53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	

Friday 15th October 2021				Date		15/10/2021																											
Street	Side	Area	Restriction	Map reference	Supply	Time																											
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM
Queen St	W	Between Shipley Ave and Sydney Trains Driveway	Unrestricted	1	3	2	2	2	2	2	2	2	2	3	2	2	2	2	2	2	2	2	2	2	2	2	2	1	1	2	1	1	1
Queen St	E	Between Shipley Ave and Wellbank St	Unrestricted	2	13	8	8	9	10	10	10	9	9	10	9	9	10	11	11	11	12	10	11	11	9	9	10	10	11	11	11	9	9
Queen St	E	Between Wellbank St and Waratah St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	9	8	4	5	12	9	10	7	12	8	8	10	8	12	11	7	8	10	10	10	7	7	9	11	7	9	8	6	3

Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM		
Queen St	W	Between Train Station Service Driveway and Power Pole	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN	4	5	0	2	1	2	4	4	3	3	3	4	2	4	3	4	4	3	5	3	4	4	3	5	4	2	3	4	3	2	0		
Queen St	W	Between Bus Stop and Train Station Service Driveway	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	5	2	3	5	3	6	2	4	7	5	6	3	4	2	3	4	3	4	6	3	5	4	2	0	5	4	2	4	1	1	1		
Queen St	W	Between Sydney Trains Driveway and Wellbank St	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	2	0	0	0	0	1	0
Queen St	W	Between Sydney Trains Driveway and Wellbank St	Disabled	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Queen St	W	Between Sydney Trains Driveway and Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	8	2	0	0	1	0	2	1	0	0	0	0	0	0	1	1	0	0	0	1	1	1	0	0	1	0	2	2	1	0	0		
Queen St	W	North of Power Pole	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	9	4	1	3	3	1	3	7	3	2	2	3	2	2	3	3	3	4	4	4	4	4	4	5	4	5	2	2	4	4	4	3	
Queen St	E	North of Waratah St	Unrestricted	10	9	1	1	2	3	4	4	3	3	3	4	3	3	3	4	4	4	4	4	3	2	2	3	3	4	4	3	3	2	2		
Wellbank St	N	Between Queen St and 115 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	11	3	1	1	1	1	2	3	3	2	3	2	3	2	4	3	2	2	2	2	2	2	2	2	1	1	3	2	2	1	0	0	
Wellbank St	S	Between Queen St and 158 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	12	1	0	0	0	1	0	1	0	0	1	0	1	0	1	1	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	
Total occupied					24	26	27	38	37	45	37	39	38	36	36	34	40	45	37	38	45	39	42	40	33	32	42	38	34	41	33	26	19			
Supply					53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	

Saturday 16th October 2021					Date	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	16/10/2021	
Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Queen St	W	Between Shipley Ave and Sydney Trains Driveway	Unrestricted	1	3	2	2	2	2	2	2	2	2	3	3	2	2	2	2	3	3	3	2	2	2	1	1	2	1	1	1	1	1	1	
Queen St	E	Between Shipley Ave and Wellbank St	Unrestricted	2	13	7	7	8	6	6	6	8	8	8	9	8	9	9	9	8	9	10	8	10	8	9	9	8	10	10	9	8	8	8	
Queen St	E	Between Wellbank St and Waratah St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	9	1	2	2	4	5	6	11	7	6	11	11	9	6	9	9	11	10	10	9	10	9	7	8	9	7	8	5	4	3	
Queen St	W	Between Train Station Service Driveway and Power Pole	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN	4	5	0	0	1	1	2	3	4	3	2	3	3	4	4	4	3	4	4	4	3	4	3	2	1	3	2	4	3	4	3	
Queen St	W	Between Bus Stop and Train Station Service Driveway	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	5	2	0	1	1	4	2	4	4	5	2	4	5	2	2	5	5	2	2	2	2	4	4	2	0	1	1	4	4	2	1	
Queen St	W	Between Sydney Trains Driveway and Wellbank St	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	6	1	0	0	0	0	0	0	1	0	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Queen St	W	Between Sydney Trains Driveway and Wellbank St	Disabled	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	
Queen St	W	Between Sydney Trains Driveway and Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	8	2	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	1	1	1	1	0	0	0	0	0	0	0	0	0	
Queen St	W	North of Power Pole	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	9	4	2	2	2	2	3	3	2	4	5	4	3	5	0	0	0	0	0	0	0	0	0	0	0	0	0	3	4	3	2	3
Queen St	E	North of Waratah St	Unrestricted	10	9	3	3	4	3	2	2	2	1	3	4	3	3	0	0	0	0	0	0	0	0	0	0	0	0	3	3	2	2	2	
Wellbank St	N	Between Queen St and 115 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	11	3	1	2	0	0	1	0	2	3	5	2	2	3	9	2	3	3	3	3	1	3	4	2	1	3	0	0	0	0	0	
Wellbank St	S	Between Queen St and 158 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	12	1	1	0	0	0	0	0	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1
Total occupied					17	19	20	22	22	25	36	35	36	40	38	37	35	34	34	34	34	29	33	33	27	21	25	23	31	35	26	23	22		
Supply					53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	

Sunday 17th October 2021					Date	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021	17/10/2021		
Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Queen St	W	Between Shipley Ave and Sydney Trains Driveway	Unrestricted	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Queen St	E	Between Shipley Ave and Wellbank St	Unrestricted	2	13	4	4	4	5	6	7	7	7	7	8	7	7	10	9	9	9	8	8	7	7	7	7	7	7	6	5	5	5	5	
Queen St	E	Between Wellbank St and Waratah St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	9	2	1	1	3	4	7	5	7	13	10	9	7	6	8	9	7	7	7	8	9	7	10	8	5	5	7	8	6	4	5
Queen St	W	Between Train Station Service Driveway and Power Pole	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN	4	5	0	0	0	1	3	4	4	4	5	3	4	3	4	4	4	3	4	5	4	3	2	3	3	4	3	2	2	0	1	
Queen St	W	Between Bus Stop and Train Station Service Driveway	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	5	2	0	0	0	0	6	2	7	6	3	4	6	5	4	4	5	3	4	3	4	3	2	3	5	4	4	3	5	3	4	3
Queen St	W	Between Sydney Trains Driveway and Wellbank St	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	1	0	0	0	0	0	0
Queen St	W	Between Sydney Trains Driveway and Wellbank St	Disabled	7	1	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Queen St	W	Between Sydney Trains Driveway and Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	8	2	0	0	0	0	0	1	1	1	1	1	1	1	1	1	1	1	1	1	0	1	1	1	1	1	1	1	1	1	1	1

Queen St	W	North of Power Pole	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	9	4	1	1	1	1	1	1	1	1	1	1	1	2	1	1	2	2	2	2	4	4	3	4	6	3	3	4	4	5	5	5
Queen St	E	North of Waratah St	Unrestricted	10	9	3	3	3	3	4	3	4	3	3	3	4	3	3	3	3	3	4	3	3	3	3	3	3	3	3	3	3	3	3	3
Wellbank St	N	Between Queen St and 115 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	11	3	1	1	1	1	1	2	3	2	4	3	5	2	5	3	2	2	2	2	2	2	2	3	2	3	2	3	2	1	2	
Wellbank St	S	Between Queen St and 158 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	12	1	1	1	1	1	1	1	1	1	2	1	0	0	1	1	1	1	1	1	1	1	1	1	1	2	1	1	1	1	1	1
Total occupied						12	11	11	15	26	28	34	32	39	34	38	29	36	35	37	31	33	34	33	30	35	37	28	32	31	33	28	24	26	
Supply						53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53

Monday 18th October 2021					Date Time		18/10/2021																												
Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Queen St	W	Between Shipley Ave and Sydney Trains Driveway	Unrestricted	1	3	1	2	2	2	2	2	2	2	2	3	3	2	2	2	2	2	2	2	2	3	3	4	3	3	2	2	1	0	0	0
Queen St	E	Between Shipley Ave and Wellbank St	Unrestricted	2	13	8	9	10	9	9	7	7	7	5	5	5	5	6	6	7	8	9	10	11	11	11	9	9	10	9	9	9	9	9	
Queen St	E	Between Wellbank St and Waratah St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	9	4	4	3	4	4	5	6	5	6	6	5	5	4	5	5	7	6	6	3	3	4	8	8	8	13	9	6	5	5	
Queen St	W	Between Train Station Service Driveway and Power Pole	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN	4	5	0	0	1	1	4	0	2	2	3	1	3	2	2	2	2	3	3	0	2	1	2	1	1	2	2	3	1	2	1	
Queen St	W	Between Bus Stop and Train Station Service Driveway	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	5	2	3	4	3	3	4	9	4	3	4	3	4	2	2	2	2	1	2	3	3	4	3	4	2	2	5	4	2	0	0	
Queen St	W	Between Sydney Trains Driveway and Wellbank St	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	6	1	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Queen St	W	Between Sydney Trains Driveway and Wellbank St	Disabled	7	1	0	0	1	1	1	1	1	1	1	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Queen St	W	Between Sydney Trains Driveway and Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	8	2	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Queen St	W	North of Power Pole	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	9	4	4	3	4	4	3	4	3	4	4	2	4	4	4	2	3	3	3	3	3	3	3	3	3	3	3	4	4	3	3	
Queen St	E	North of Waratah St	Unrestricted	10	9	2	1	1	1	1	1	1	1	1	1	1	1	2	2	4	3	3	2	4	2	3	6	4	3	4	4	2	2	1	
Wellbank St	N	Between Queen St and 115 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	11	3	1	1	2	5	2	3	3	2	3	4	3	3	2	1	2	2	2	2	2	2	2	2	2	3	2	3	3	2	3	1
Wellbank St	S	Between Queen St and 158 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	12	1	1	1	1	0	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	1	1	0	1	1	1	1	1	1	1	1
Total occupied						24	25	28	31	30	32	30	28	28	29	28	27	25	22	27	29	30	28	33	30	33	38	35	34	45	39	28	26	22	
Supply						53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53

Tuesday 19th October 2021					Date Time		19/10/2021																												
Street	Side	Area	Restriction	Map reference	Supply	6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Queen St	W	Between Shipley Ave and Sydney Trains Driveway	Unrestricted	1	3	2	2	3	3	3	3	3	3	3	3	3	3	3	4	4	3	2	2	2	2	2	2	2	1	1	1	1	0	0	
Queen St	E	Between Shipley Ave and Wellbank St	Unrestricted	2	13	11	11	11	12	11	11	9	11	10	10	10	9	10	10	10	10	11	10	10	10	10	8	9	9	11	9	8	6	4	5
Queen St	E	Between Wellbank St and Waratah St	1P 830AM-6PM MON-FRI, 830AM-1230PM SAT	3	9	2	6	9	4	4	4	6	6	6	5	4	5	6	5	5	7	8	5	6	7	5	8	6	6	6	6	6	6	5	
Queen St	W	Between Train Station Service Driveway and Power Pole	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN	4	5	4	0	2	5	4	4	2	3	2	3	1	2	4	4	3	1	2	3	2	3	3	3	2	3	2	4	1	1	1	
Queen St	W	Between Bus Stop and Train Station Service Driveway	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	5	2	1	4	2	3	4	2	5	4	4	5	3	2	4	4	4	5	3	3	1	3	4	3	2	2	4	1	1	1	1	
Queen St	W	Between Sydney Trains Driveway and Wellbank St	1P 930AM-330PM MON-FRI, 930AM-1230PM SAT-SUN, NP 630AM-930AM, 330PM-	6	1	0	0	0	0	0	0	0	0	0	0	1	0	0	2	1	1	1	1	1	1	1	1	2	1	1	2	0	1	0	0
Queen St	W	Between Sydney Trains Driveway and Wellbank St	Disabled	7	1	0	0	0	0	1	0	1	1	1	1	1	1	1	1	1	1	1	0	1	1	0	1	1	1	1	1	1	1	1	0
Queen St	W	Between Sydney Trains Driveway and Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	8	2	0	0	0	0	0	0	0	0	1	1	0	0	1	1	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0
Queen St	W	North of Power Pole	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	9	4	3	1	2	2	3	2	2	3	3	2	3	3	4	4	3	2	2	3	4	2	3	5	5	4	4	5	5	4	4	
Queen St	E	North of Waratah St	Unrestricted	10	9	4	4	5	5	5	5	3	4	5	5	5	5	5	4	4	4	5	5	5	3	2	2	2	2	2	2	3	3		
Wellbank St	N	Between Queen St and 115 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	11	3	2	3	2	2	2	3	4	2	2	2	2	2	3	3	4	2	2	3	2	3	2	3	3	1	2	0	0	1	1	
Wellbank St	S	Between Queen St and 158 Wellbank St	2P 830AM-6PM MON-FRI, 830AM-12PM SAT	12	1	1	1	1	1	2	1	1	1	1	1	1	1	1	1	1	1	1	0	0	0	1	0	0	0	1	1	1	1	1	
Total occupied						30	32	37	37	38	36	36	38	38	39	33	33	44	42	42	37	37	35	36	35	32	36	34	31	35	27	28	22	21	
Supply						53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53	53



Legend
Location of survey
Map reference

Monday 18th October 2021				Date		18/10/2021																												
Street	Side	Area	Restriction	Map reference	Supply	Time																												
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM
Herb Elliot Dr	S	Across road from Discobolus	Untimed Ticketed Parking	1	7	0	0	0	0	1	2	3	6	5	3	3	7	6	7	6	7	4	4	6	4	7	7	4	5	7	7	7	7	6
Herb Elliot Dr	S	Across from Sydney International Athletics Centre	Untimed Ticketed Parking	2	8	0	0	0	0	1	2	3	5	5	4	3	5	5	6	6	5	3	1	3	2	1	4	2	1	4	4	7	6	6
Herb Elliot Dr	S	Between Olympic Blvd and across road from Sydney Olympic Park Business Association	P2min & P15min	3	12	0	2	1	2	3	5	6	6	6	6	6	7	6	6	6	5	3	3	3	2	3	5	6	6	4	6	3	4	
Total occupied					0	2	1	2	5	9	12	17	16	13	12	18	18	19	18	18	12	8	12	9	10	14	11	12	17	15	20	16	16	
Supply					27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27

Tuesday 19th October 2021				Date		19/10/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																													
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Herb Elliot Dr	S	Across road from Discobolus	Untimed Ticketed Parking	1	7	1	1	2	1	2	5	3	3	3	5	6	7	5	7	7	7	6	5	5	5	6	7	5	6	3	4	4	3	0	
Herb Elliot Dr	S	Across from Sydney International Athletics Centre	Untimed Ticketed Parking	2	8	0	0	0	0	0	1	1	3	3	3	3	2	2	7	8	3	5	3	1	3	5	6	3	1	0	0	3	3	2	
Herb Elliot Dr	S	Between Olympic Blvd and across road from Sydney Olympic Park Business Association	P2min & P15min	3	12	1	3	3	4	5	5	5	5	7	7	4	6	4	6	6	4	3	2	4	2	3	4	4	3	1	6	6	4	4	
Total occupied					2	4	5	5	7	11	9	11	13	15	13	15	11	20	21	14	14	10	10	10	14	17	12	10	4	10	13	10	6		
Supply					27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27

Wednesday 20th October 2021				Date		20/10/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																													
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Herb Elliot Dr	S	Across road from Discobolus	Untimed Ticketed Parking	1	7	1	1	1	1	4	5	6	2	5	7	6	7	7	7	4	5	6	4	7	4	5	6	3	2	2	1	3	6	2	
Herb Elliot Dr	S	Across from Sydney International Athletics Centre	Untimed Ticketed Parking	2	8	0	0	0	0	0	0	0	3	4	7	5	5	7	6	7	5	4	2	4	3	3	2	1	1	1	1	1	1	1	
Herb Elliot Dr	S	Between Olympic Blvd and across road from Sydney Olympic Park Business Association	P2min & P15min	3	12	1	1	3	7	7	7	7	7	7	7	6	7	7	7	5	7	6	6	6	7	6	7	3	5	5	5	6	7		
Total occupied					2	2	4	8	11	12	13	12	16	21	18	18	21	20	18	15	17	12	17	13	15	14	11	6	8	7	9	13	10		
Supply					27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27

Thursday 21st October 2021				Date		21/10/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																													
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Herb Elliot Dr	S	Across road from Discobolus	Untimed Ticketed Parking	1	7	0	0	2	0	3	2	4	6	5	6	6	3	3	2	5	3	5	4	1	3	1	0	2	1	2	3	4	3	0	
Herb Elliot Dr	S	Across from Sydney International Athletics Centre	Untimed Ticketed Parking	2	8	0	0	0	0	1	2	3	5	4	5	4	6	5	3	2	4	4	3	5	5	5	4	4	2	0	0	0	0	0	
Herb Elliot Dr	S	Between Olympic Blvd and across road from Sydney Olympic Park Business Association	P2min & P15min	3	12	4	3	3	4	6	7	7	6	6	4	4	4	4	4	6	3	4	5	4	4	3	6	7	7	5	4	4	2	2	
Total occupied					4	3	5	4	10	11	14	17	15	14	13	12	9	13	10	13	12	10	12	9	10	13	10	7	7	8	5	2	2	2	
Supply					27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27

Friday 22nd October 2021				Date		22/10/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																													
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Herb Elliot Dr	S	Across road from Discobolus	Untimed Ticketed Parking	1	7	0	0	1	1	3	6	6	5	6	6	5	6	5	6	6	5	4	4	6	6	3	5	4	3	4	3	5	6	2	
Herb Elliot Dr	S	Across from Sydney International Athletics Centre	Untimed Ticketed Parking	2	8	0	0	0	0	0	0	1	2	4	2	3	5	4	3	3	5	3	1	1	1	0	0	1	0	1	1	2	2	3	
Herb Elliot Dr	S	Between Olympic Blvd and across road from Sydney Olympic Park Business Association	P2min & P15min	3	12	1	2	3	6	4	4	6	6	7	7	6	6	3	3	5	4	2	3	5	3	1	2	4	5	5	6	4	4		
Total occupied					1	2	4	7	7	10	13	13	17	15	15	17	15	12	12	15	11	7	10	12	6	6	7	7	10	9	13	12	9		
Supply					27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27

Saturday 23rd October 2021				Date		23/10/2021																													
Street	Side	Area	Restriction	Map reference	Supply	Time																													
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM	8PM	
Herb Elliot Dr	S	Across road from Discobolus	Untimed Ticketed Parking	1	7	0	0	0	1	3	7	1	4	4	5	4	4	5	3	5	5	3	0	2	5	2	3	0	4	4	6	6	6	6	
Herb Elliot Dr	S	Across from Sydney International Athletics Centre	Untimed Ticketed Parking	2	8	0	0	0	0	0	0	2	2	2	2	4	3	2	3	5	6	3	4	3	1	2	0	1	1	1	2	3	4		
Herb Elliot Dr	S	Between Olympic Blvd and across road from Sydney Olympic Park Business Association	P2min & P15min	3	12	2	3	3	4	2	1	2	2	4	4	6	7	4	7	6	7	2	2	3	5	5	5	2	5	4	3	5	5		
Total occupied					2	3	3	5	5	8	5	8	10	11	14	14	11	13	14	17	11	5	9	13	8	10	2	10	9	10	13	14	15		
Supply					27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27	27

Sunday 24th October 2021				Date		24/10/2021																											
Street	Side	Area	Restriction	Map reference	Supply	Time																											
						6AM	630AM	7AM	730AM	8AM	830AM	9AM	930AM	10AM	1030AM	11AM	1130AM	12PM	1230PM	1PM	130PM	2PM	230PM	3PM	330PM	4PM	430PM	5PM	530PM	6PM	630PM	7PM	730PM

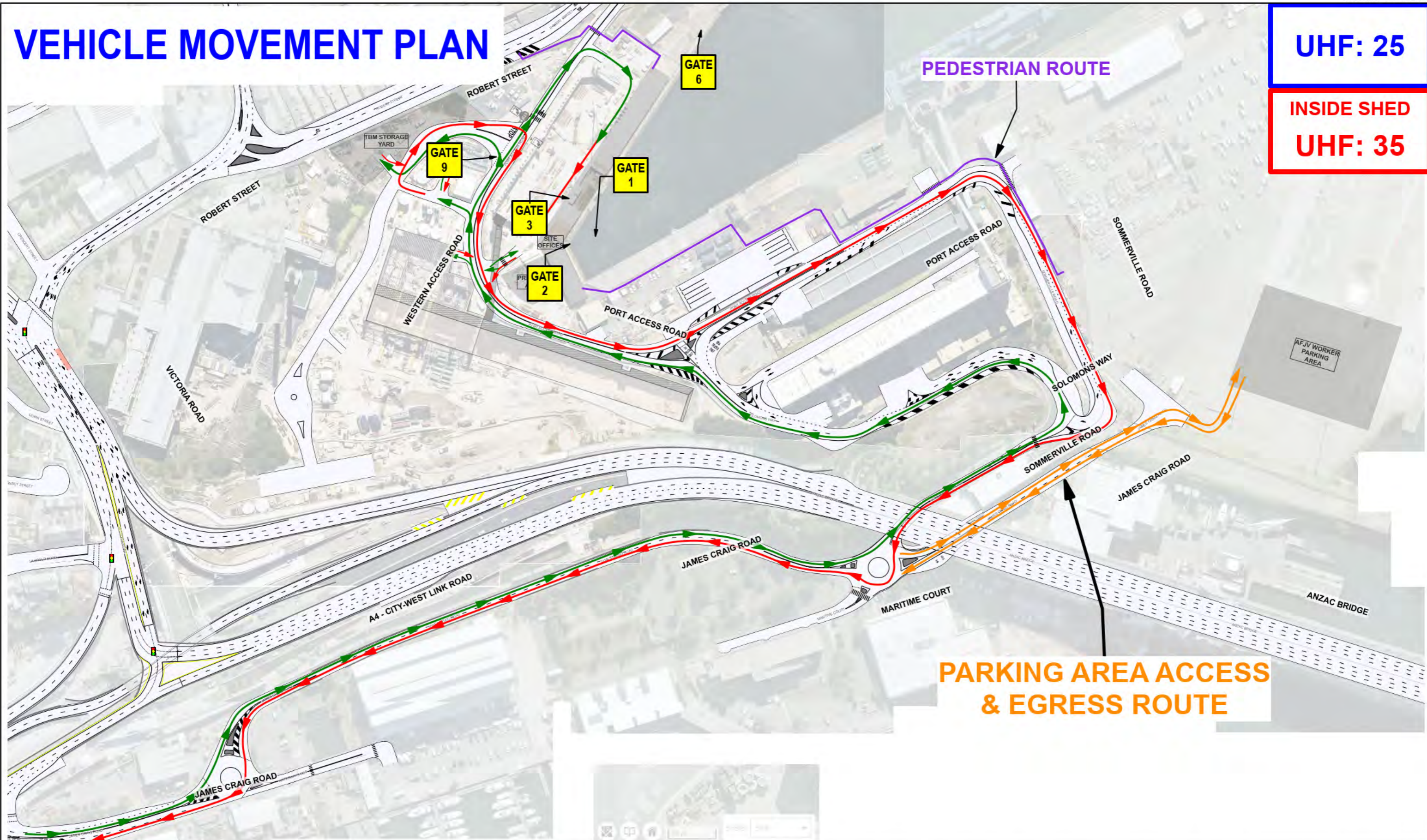
6.2 SAMPLE HEAVY VEHICLE ROUTES (VEHICLE MOVEMENT PLANS)

VEHICLE MOVEMENT PLAN

UHF: 25

INSIDE SHED

UHF: 35



PARKING AREA ACCESS & EGRESS ROUTE

Date: 31/05/2024 **Location:** The Bay Station **Author name:**

Comments:

- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow
 7. follow all road rules and speed limits.
- Use only approved haul routes



PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE		REV - 00	
LEGEND			
	Boundaries		Signalised intersection
	Access		Parking Access/Egress
	Egress		Pedestrian Route



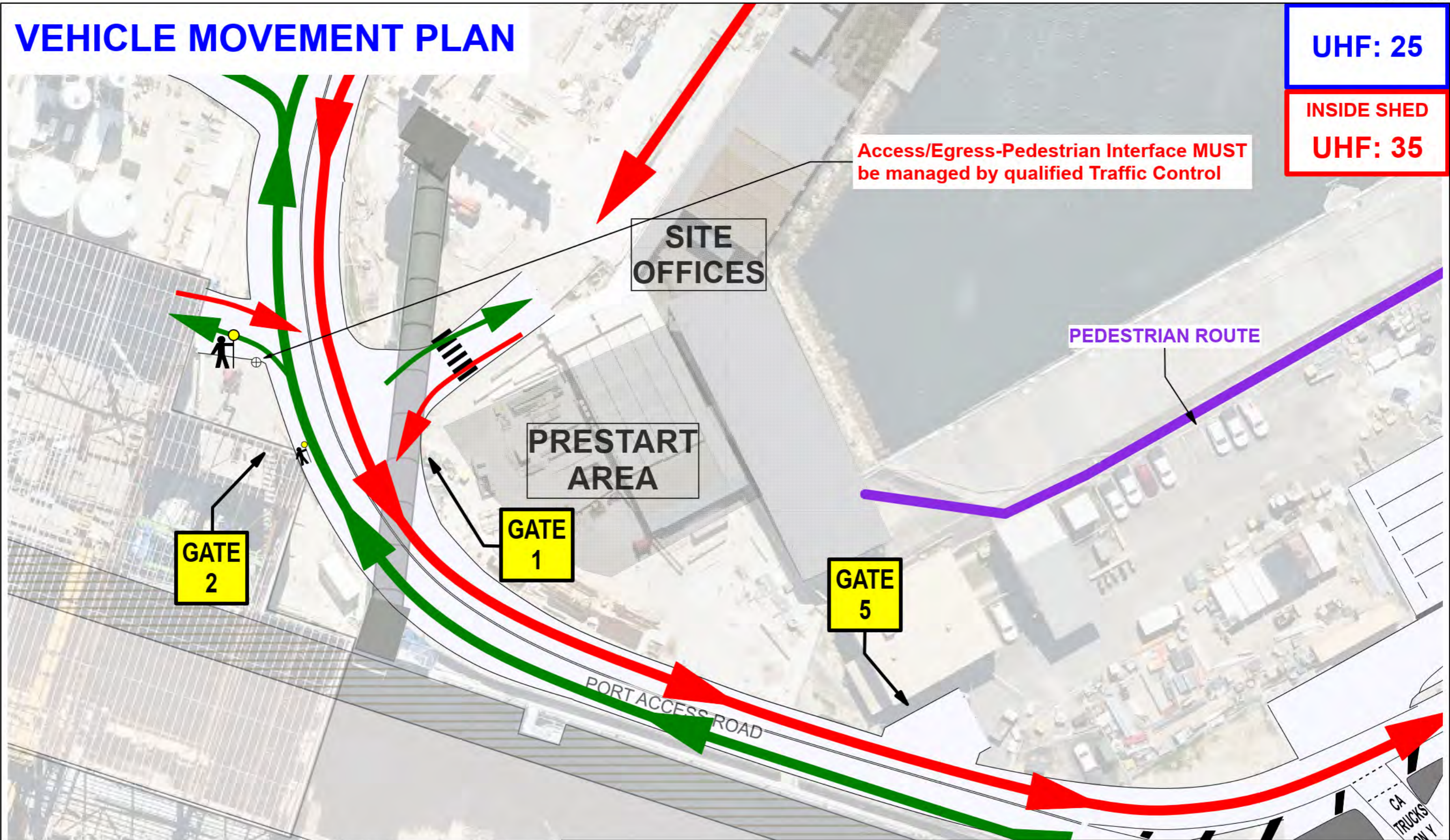
VEHICLE MOVEMENT PLAN

UHF: 25

INSIDE SHED

UHF: 35

Access/Egress-Pedestrian Interface MUST be managed by qualified Traffic Control



Date: 31/05/2024 Location: The Bay Station Author name:

Comments:

- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intension via UHF
 3. Indicate intensions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes



PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

REV - 00

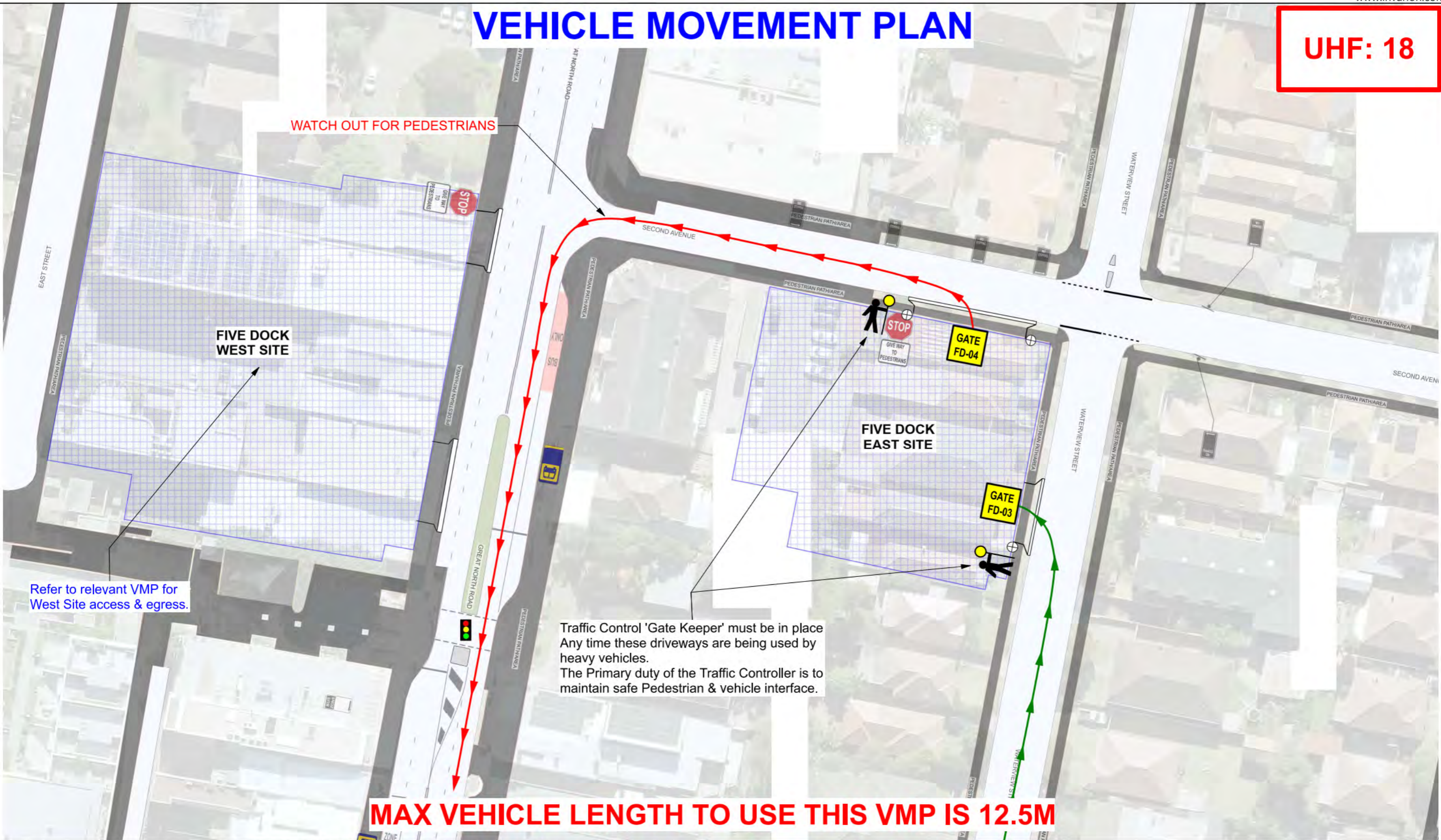
LEGEND

	Boundaries		Signalised intersection
	Access		Parking Access/Egress
	Egress		Pedestrian Route



VEHICLE MOVEMENT PLAN

UHF: 18



MAX VEHICLE LENGTH TO USE THIS VMP IS 12.5M

Refer to relevant VMP for West Site access & egress.

WATCH OUT FOR PEDESTRIANS

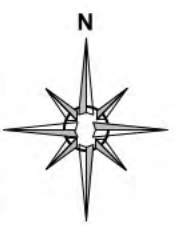
Traffic Control 'Gate Keeper' must be in place Any time these driveways are being used by heavy vehicles. The Primary duty of the Traffic Controller is to maintain safe Pedestrian & vehicle interface.



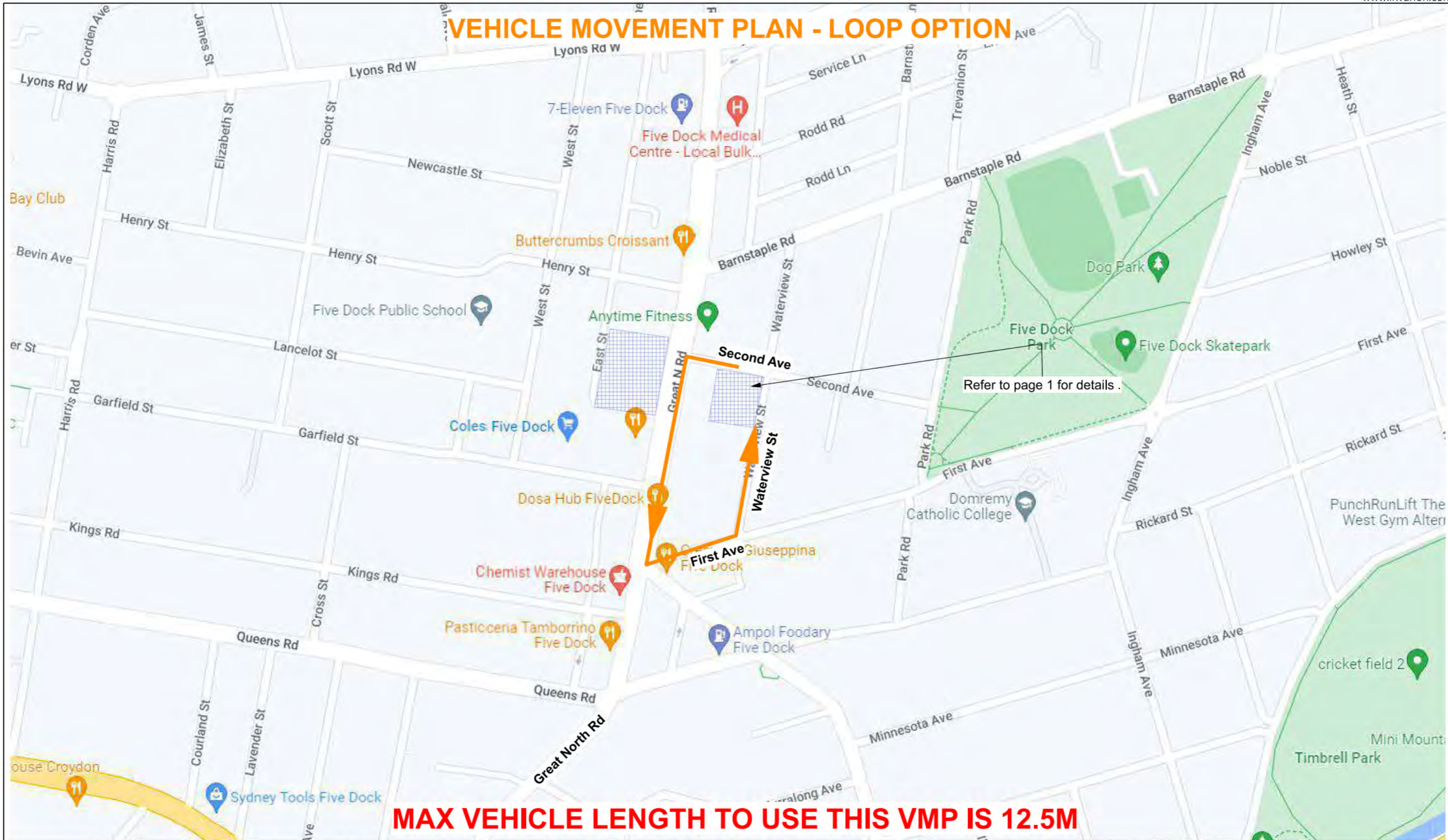
Date: 12/01/2023 **Location:** Five Dock Station - Eastern Site **Author name:**

- Comments:**
- Drivers must be briefed on this VMP
 - Gatekeeper/s must be in position when gates are in use and the VMP requires it.
 - Drivers must adhere to Gatekeepers directions
 - Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intension via UHF
 3. Indicate intensions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
 - Use only approved haul routes

PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE		REV - 00	
LEGEND			
	Station Site Boundary		Signalled Intersection
	Access		Loop Option
	Egress		



VEHICLE MOVEMENT PLAN - LOOP OPTION



MAX VEHICLE LENGTH TO USE THIS VMP IS 12.5M

Date: 12/01/2023 **Location:** Five Dock Station - Eastern Site **Author name:**

Comments:

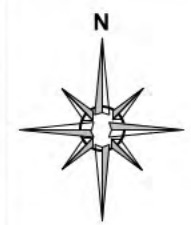
- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes



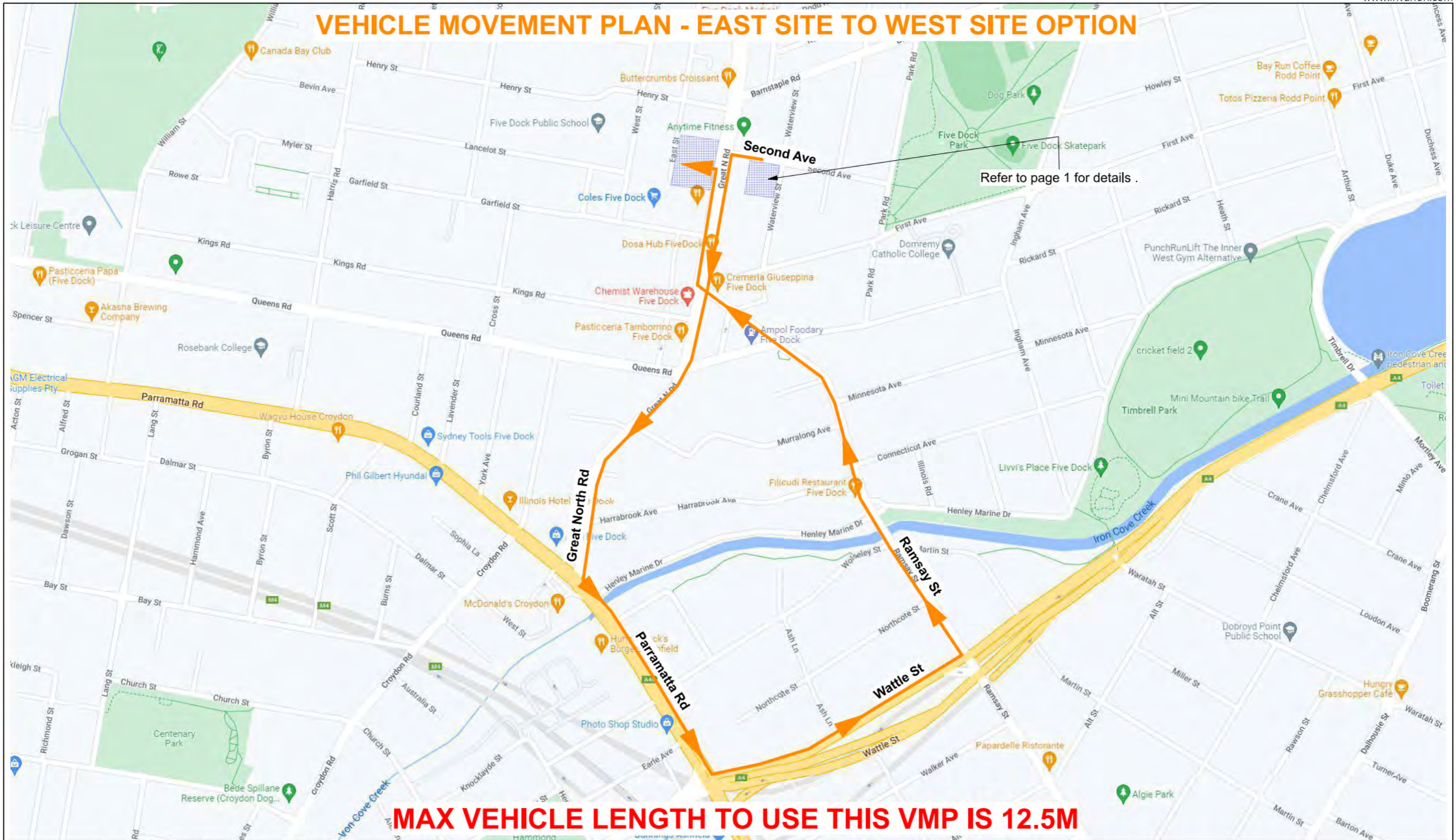
PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Station Site Boundary		Signalised Intersection
	Access		Loop Option
	Egress		



VEHICLE MOVEMENT PLAN - EAST SITE TO WEST SITE OPTION



MAX VEHICLE LENGTH TO USE THIS VMP IS 12.5M

Date: 12/01/2023 **Location:** Five Dock Station - Eastern Site **Author name:** [Redacted]

Comments:

- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intension via UHF
 3. Indicate intensions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes



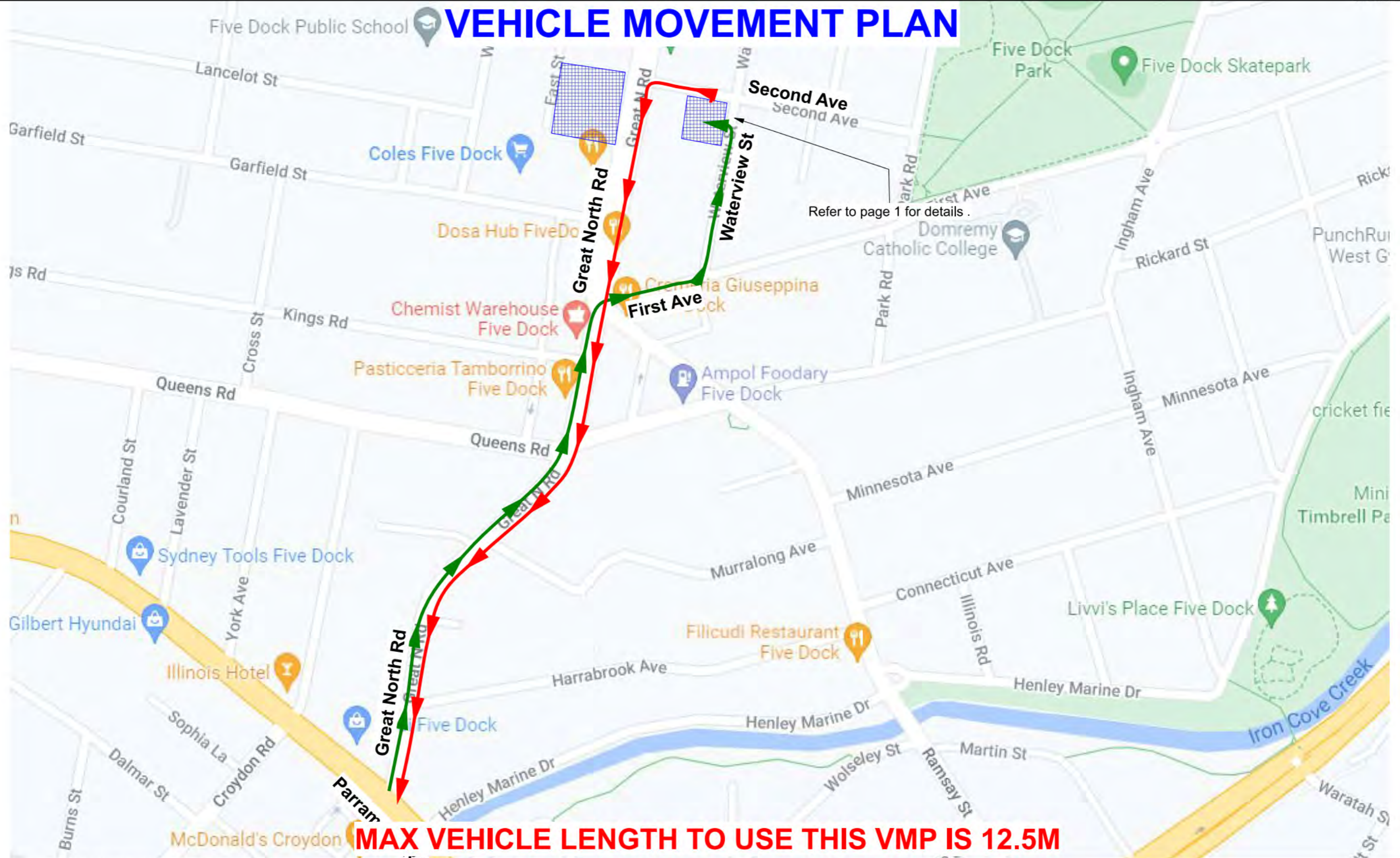
PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Station Site Boundary		Signalised Intersection
	Access		Loop Option
	Egress		



VEHICLE MOVEMENT PLAN



MAX VEHICLE LENGTH TO USE THIS VMP IS 12.5M

Date: 12/01/2023 **Location:** Five Dock Station - Eastern Site **Author name:** [REDACTED]

Comments:

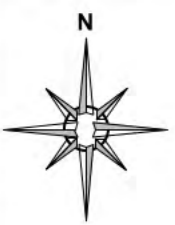
- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes



PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Workzone		Signalised intersection
	Access		Restricted/other movement
	Egress		

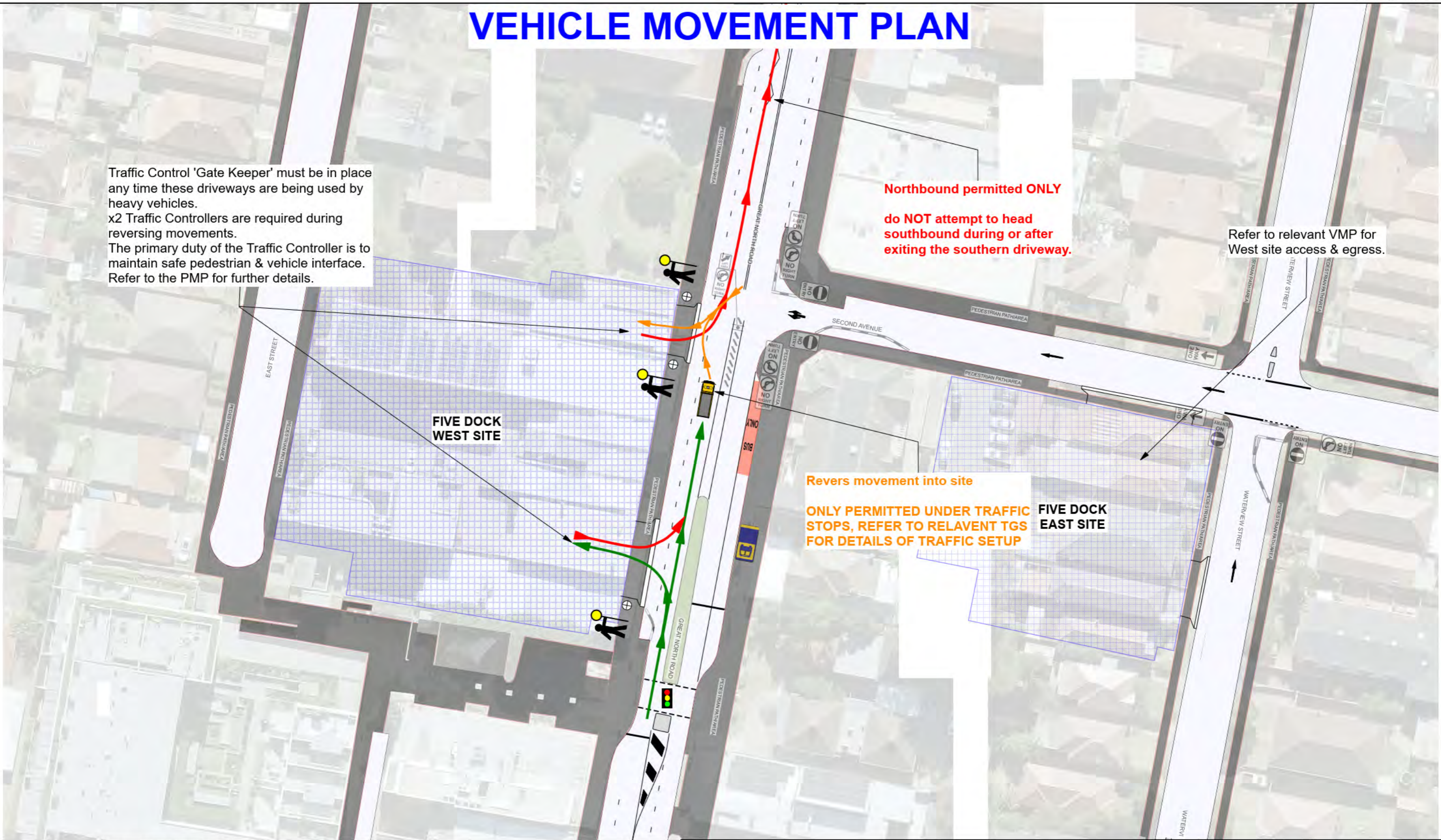


VEHICLE MOVEMENT PLAN

Traffic Control 'Gate Keeper' must be in place any time these driveways are being used by heavy vehicles.
 x2 Traffic Controllers are required during reversing movements.
 The primary duty of the Traffic Controller is to maintain safe pedestrian & vehicle interface.
 Refer to the PMP for further details.

Northbound permitted ONLY
 do NOT attempt to head southbound during or after exiting the southern driveway.

Refer to relevant VMP for West site access & egress.



FIVE DOCK WEST SITE

FIVE DOCK EAST SITE

Revers movement into site
ONLY PERMITTED UNDER TRAFFIC STOPS, REFER TO RELEVANT TGS FOR DETAILS OF TRAFFIC SETUP

Date: 06/08/2024 **Location:** Five Dock - Western Site

Author name:

Comments:

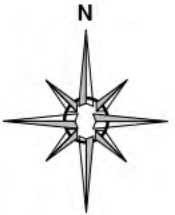
- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intension via UHF
 3. Indicate intensions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow
 7. follow all road rules and speed limits.
- Use only approved haul routes



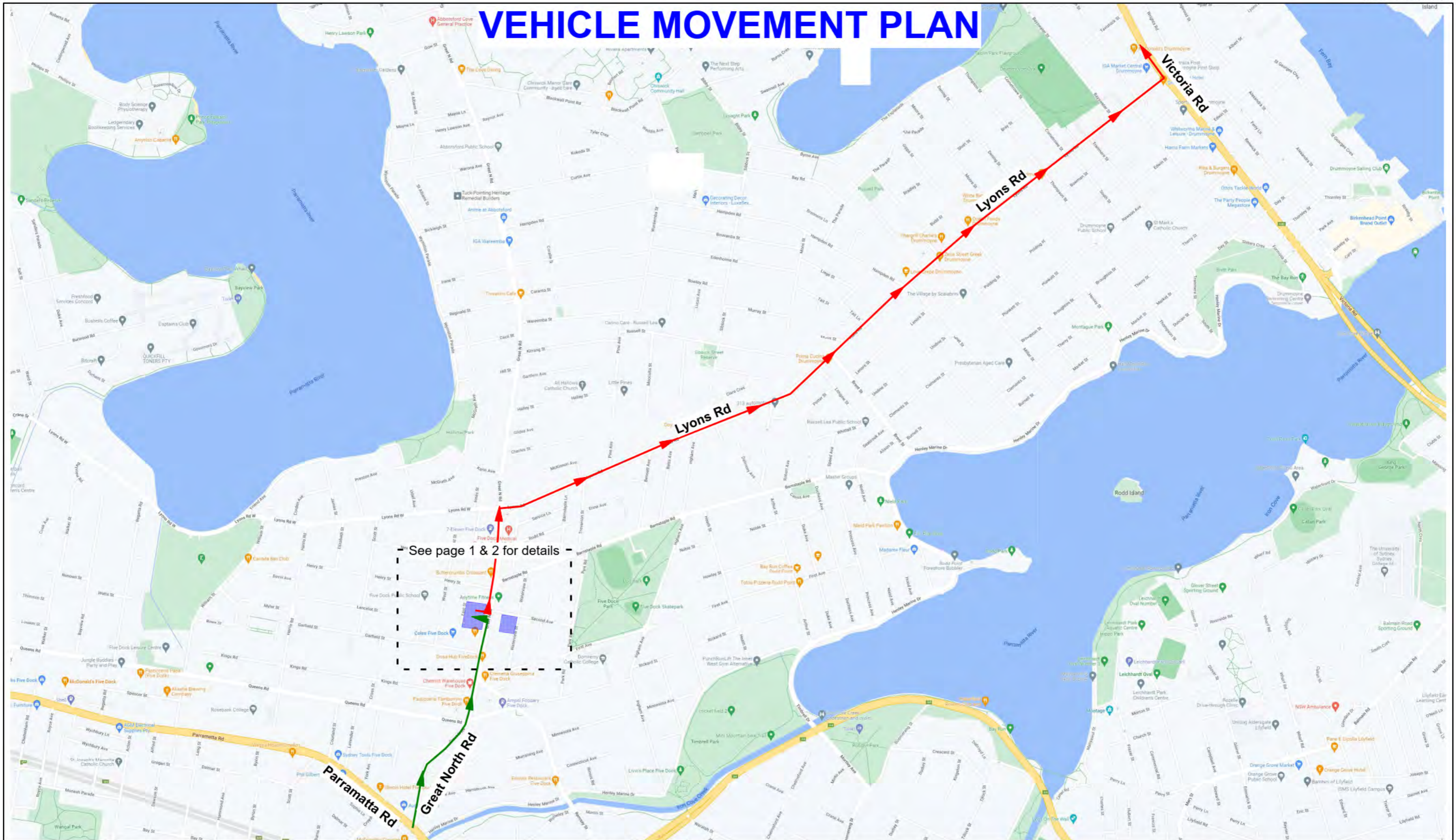
PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Workzone		Signalised intersection
	Access		Special movement
	Egress		



VEHICLE MOVEMENT PLAN



See page 1 & 2 for details

Date: 06/08/2024 **Location:** Five Dock - Western Site **Author name:**

Comments:

- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes

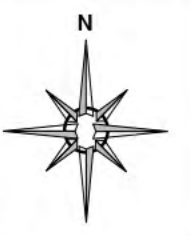


PROJECT:

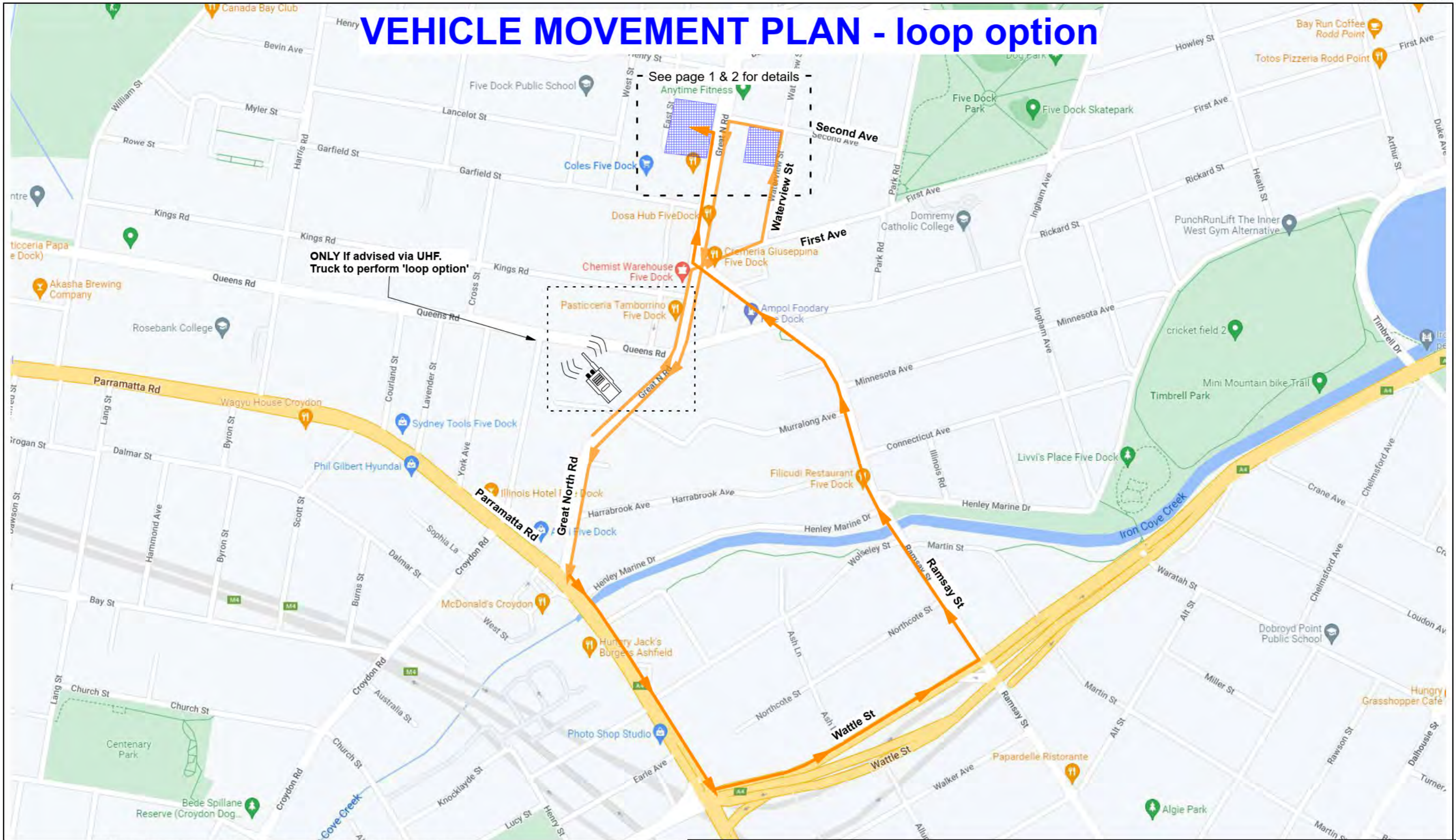
SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Workzone		Signalised intersection
	Access		Special movement
	Egress		



VEHICLE MOVEMENT PLAN - loop option



Date: 06/08/2024 **Location:** Five Dock - Western Site **Author name:**

Comments:

- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes



PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

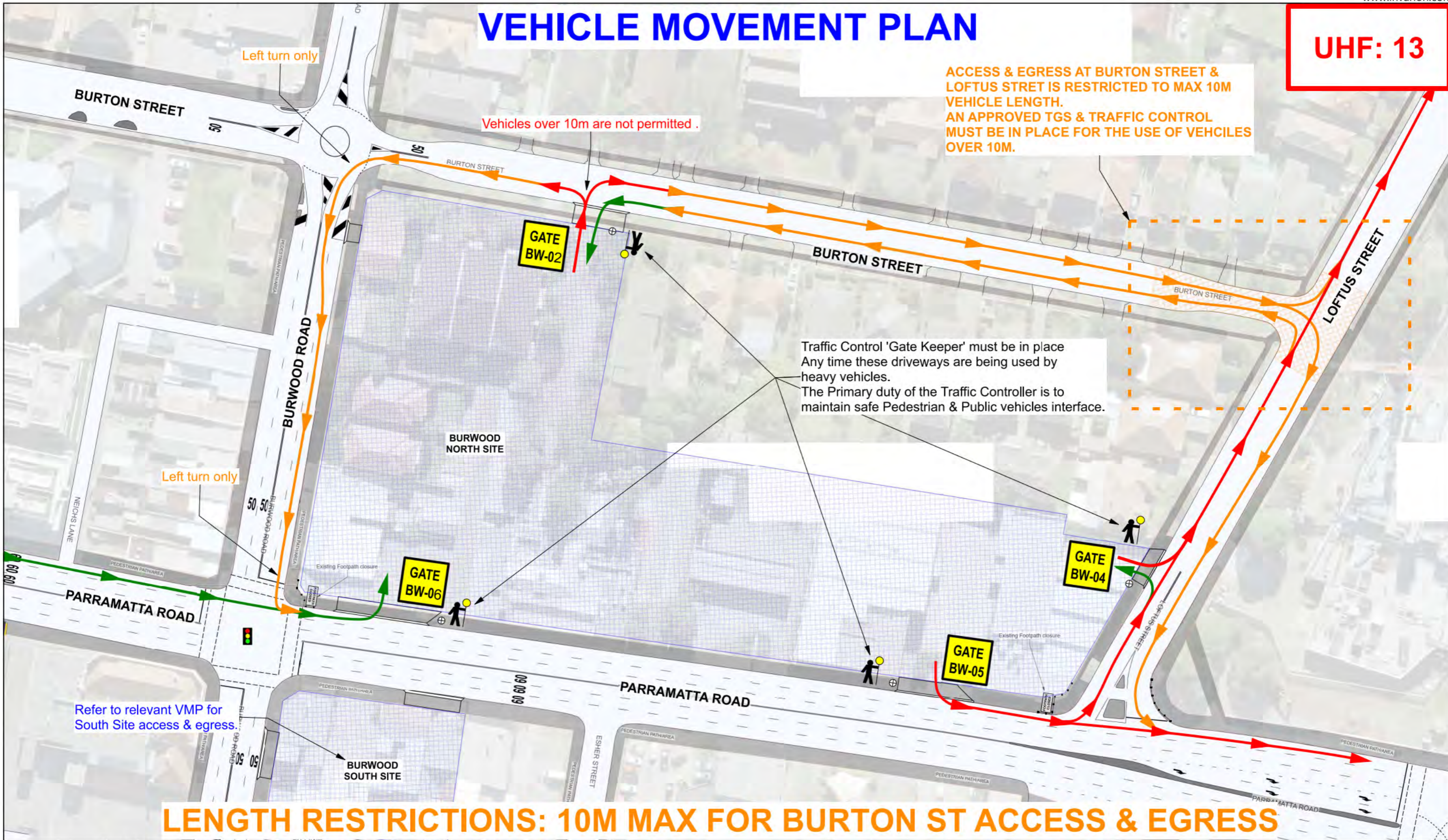
LEGEND

	Workzone		Signalised intersection
	Access		Special movement
	Egress		



VEHICLE MOVEMENT PLAN

UHF: 13



LENGTH RESTRICTIONS: 10M MAX FOR BURTON ST ACCESS & EGRESS

Date: 25/05/2023 **Location:** Burwood Station - Northern Site **Author name:**

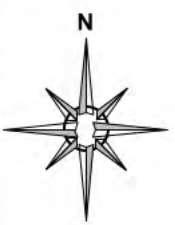
- Comments:**
- Drivers must be briefed on this VMP
 - Gatekeeper/s must be in position when gates are in use and the VMP requires it.
 - Drivers must adhere to Gatekeepers directions
 - Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
 - Use only approved haul routes



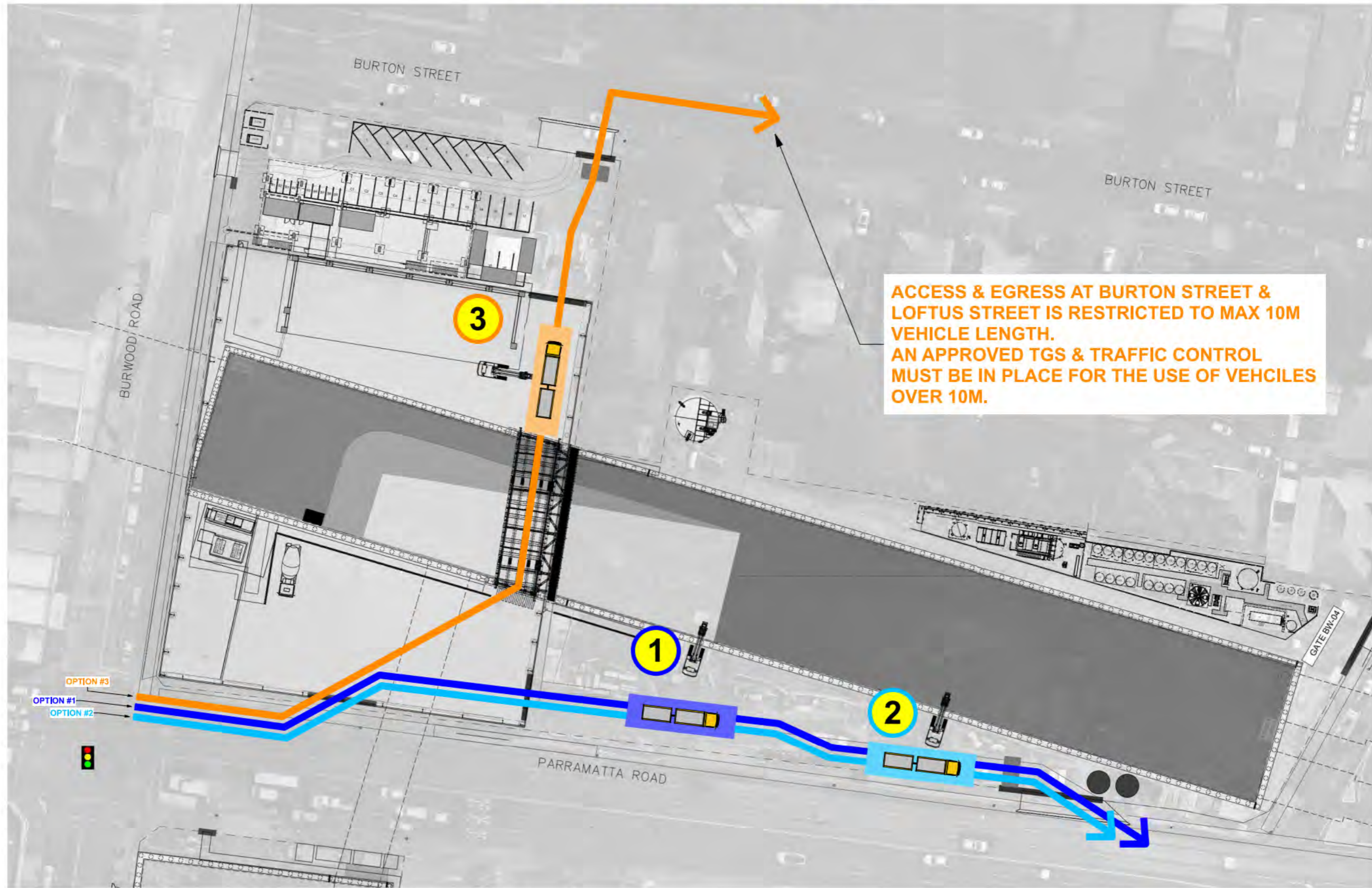
PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Station Site Boundary		Signalised Intersection
	Access		Restricted Movement
	Egress		Qualified Gatekeeper



SITE SPOIL AREAS



Date: 25/05/2023 **Location:** Burwood Station - Northern Site **Author name:**

Comments:

- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes

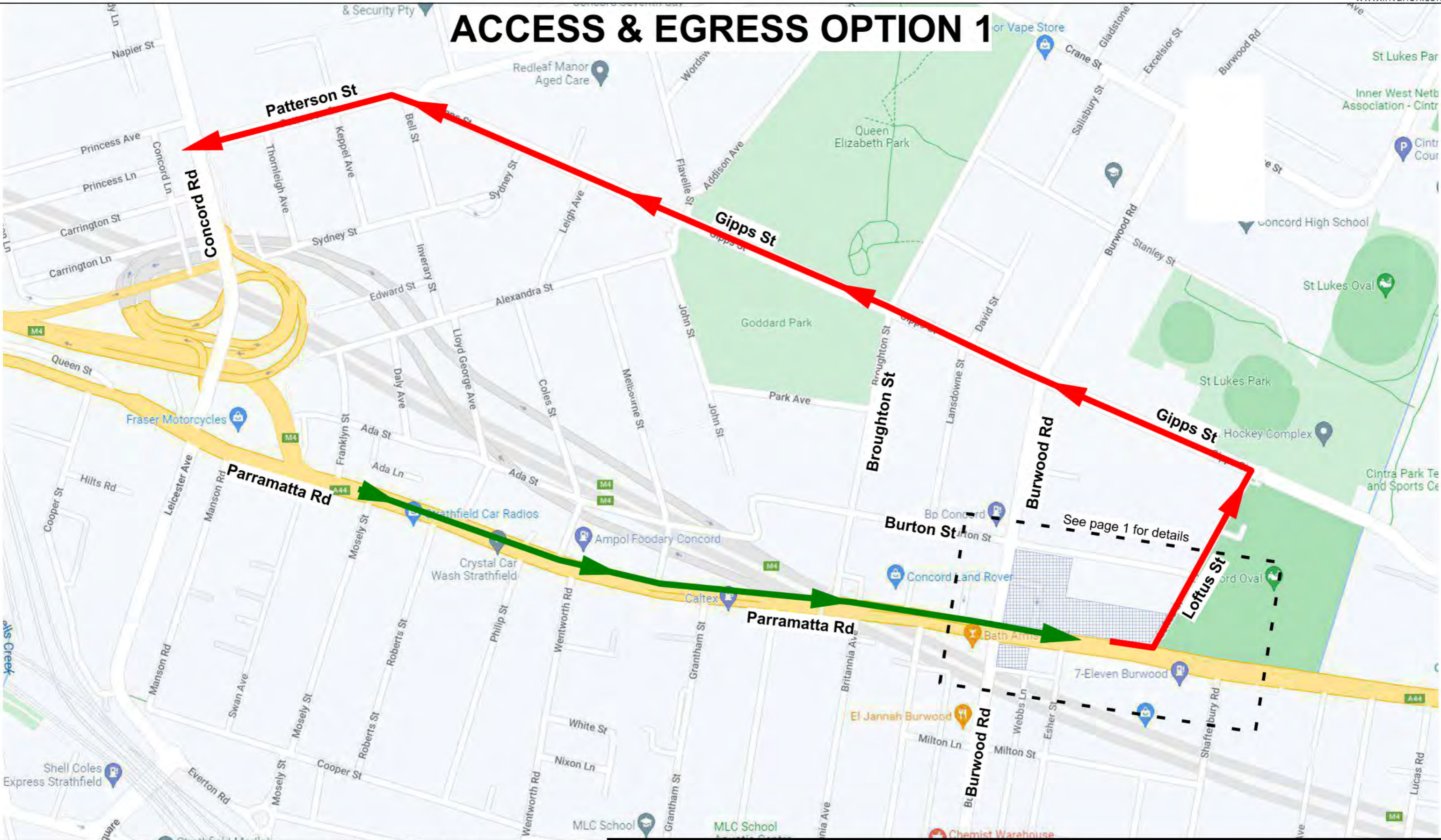
PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

① ② ③	Spoil Area Locations	🚦	Signalised intersection
→ (Blue)	Option #1	→ (Orange)	Option #3 (Restricted movement)
→ (Light Blue)	Option #2		



ACCESS & EGRESS OPTION 1



Date: 25/05/2023 **Location:** Burwood Station - Northern Site **Author name:** [Redacted]

Comments:

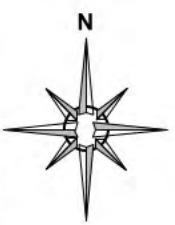
- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes



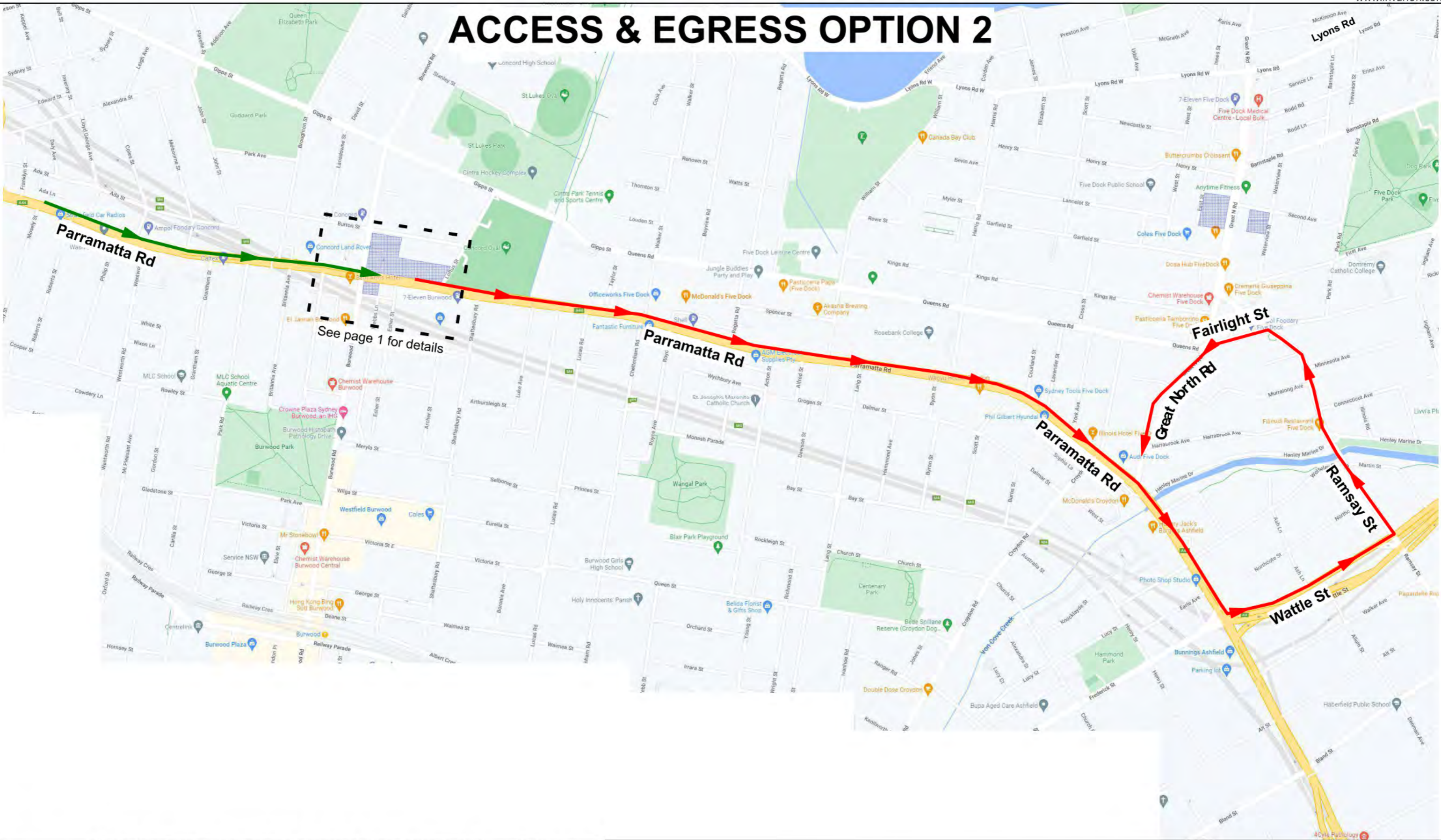
PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Station Site Boundary		Signalised intersection
	Access		
	Egress		



ACCESS & EGRESS OPTION 2



Date: 25/05/2023 **Location:** Burwood Station - Northern Site **Author name:** [Redacted]

Comments:

- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes



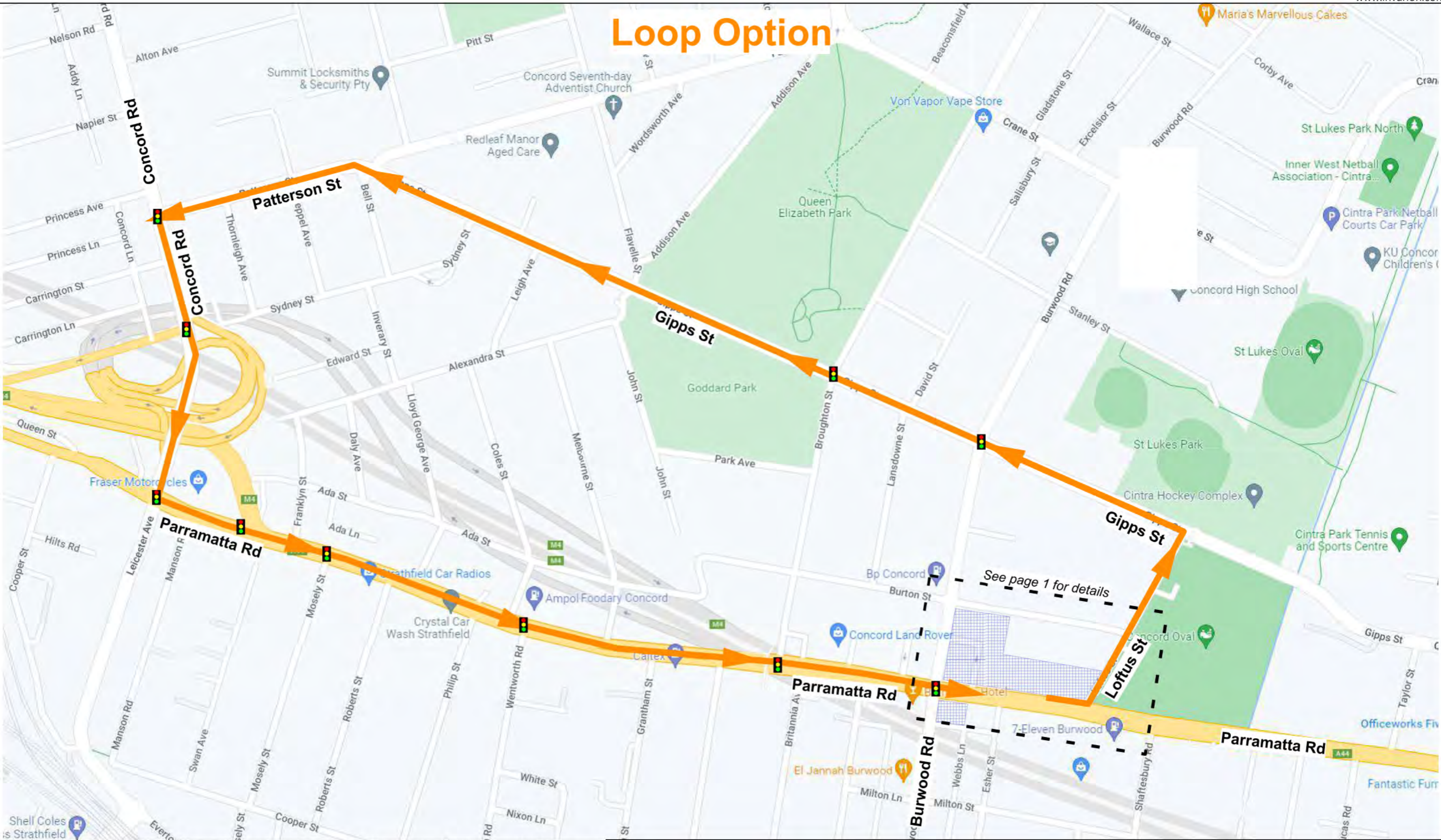
PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Station Site Boundary		Signalised intersection
	Access		
	Egress		



Loop Option



Date: 25/05/2023 **Location:** Burwood Station - Northern Site **Author name:** [REDACTED]

Comments:

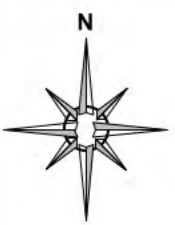
- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes



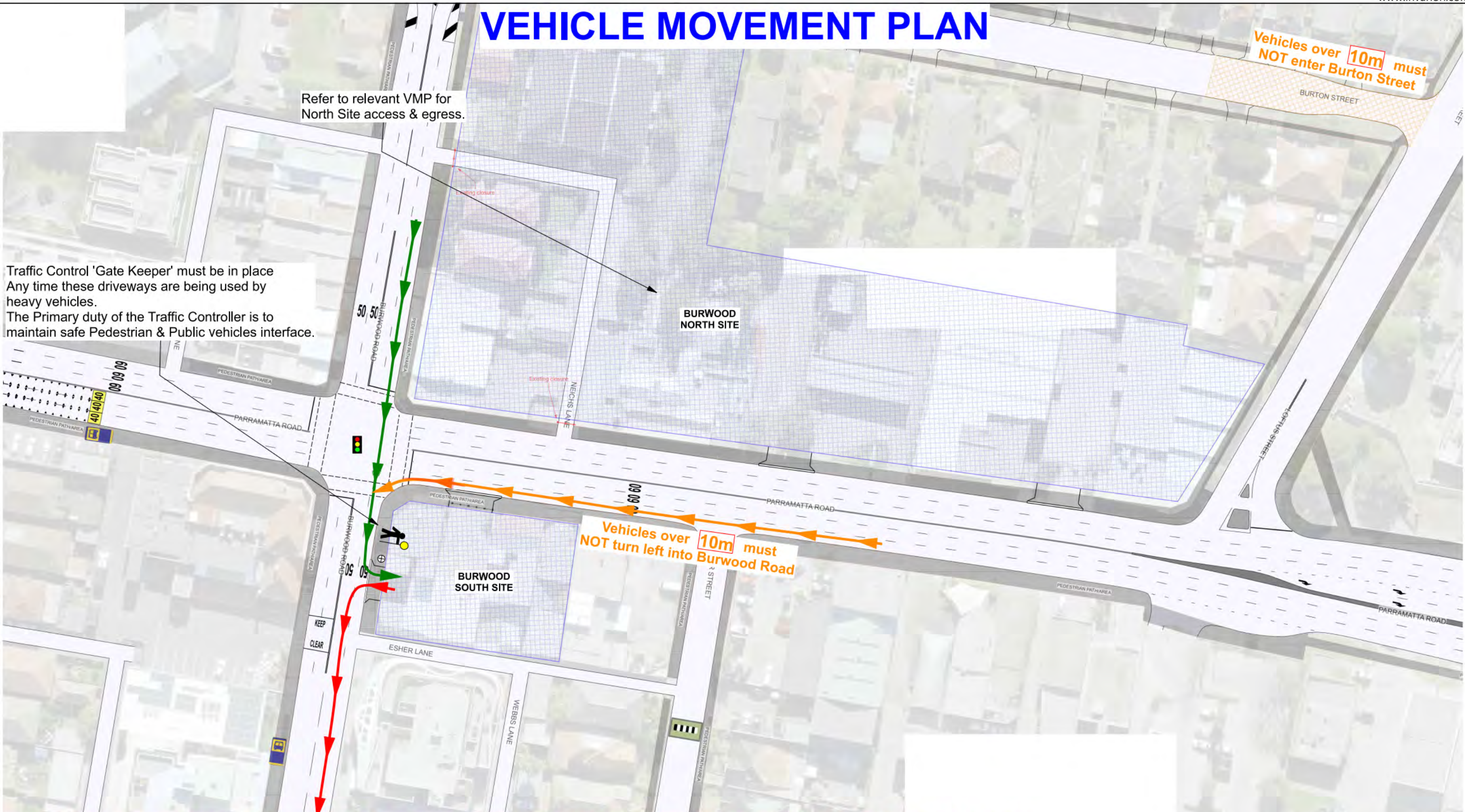
PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Station Site Boundary		Signalised intersection
	Access		Loop Option
	Egress		



VEHICLE MOVEMENT PLAN



LENGTH RESTRICTIONS: 10M MAX FOR VEHICLES ENTERING SOUTH SITE

Date: 15/06/2023 **Location:** Burwood - Southern Site

Author name:





Comments:

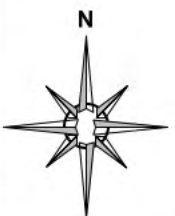
- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes

PROJECT:

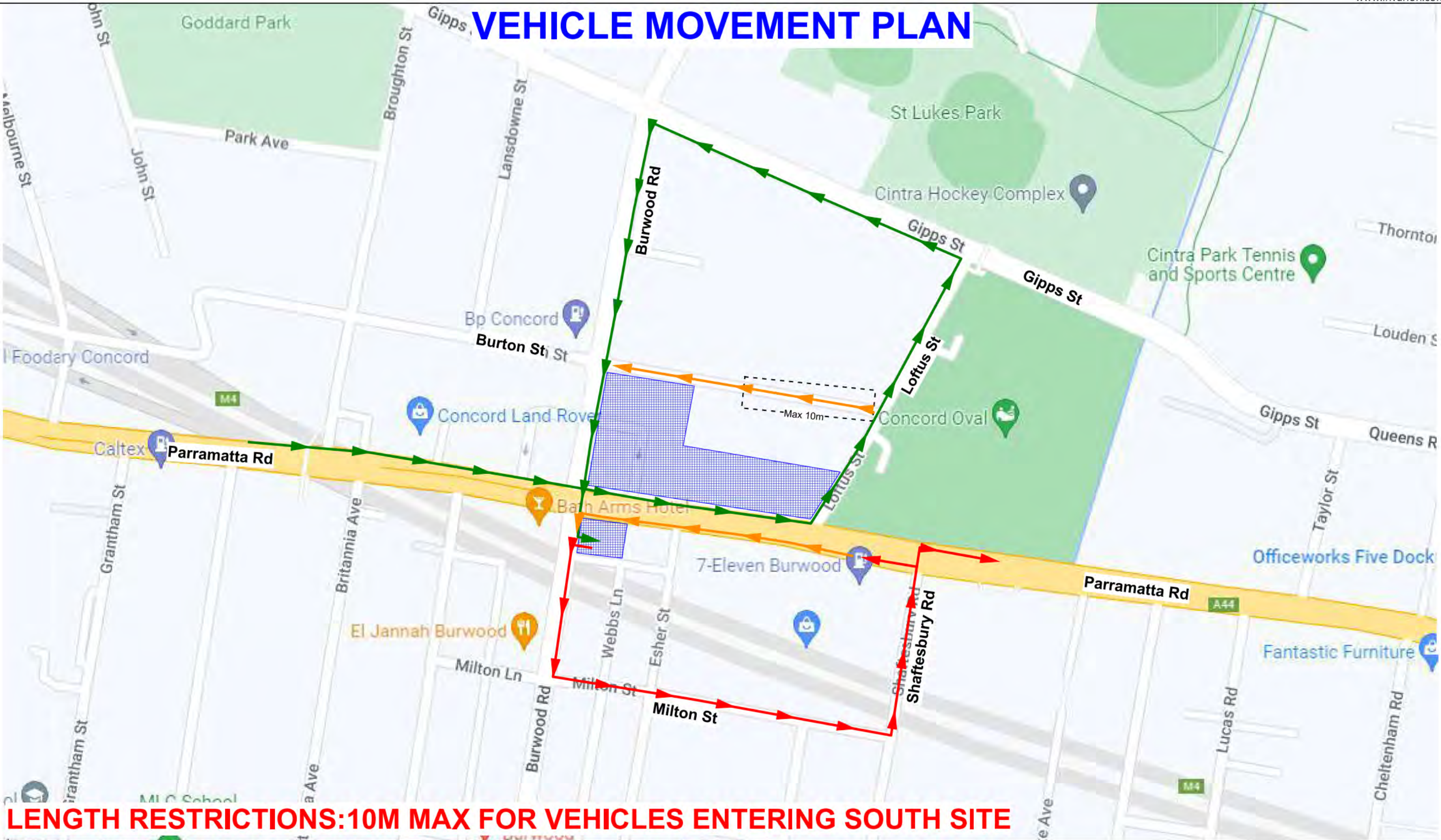
SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Workzone		Signalised intersection
	Access		Restricted movement
	Egress		



VEHICLE MOVEMENT PLAN








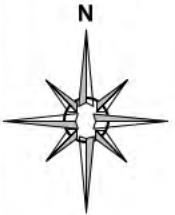
LENGTH RESTRICTIONS: 10M MAX FOR VEHICLES ENTERING SOUTH SITE

Date: 15/06/2023 Location: Burwood - Southern Site Author name:

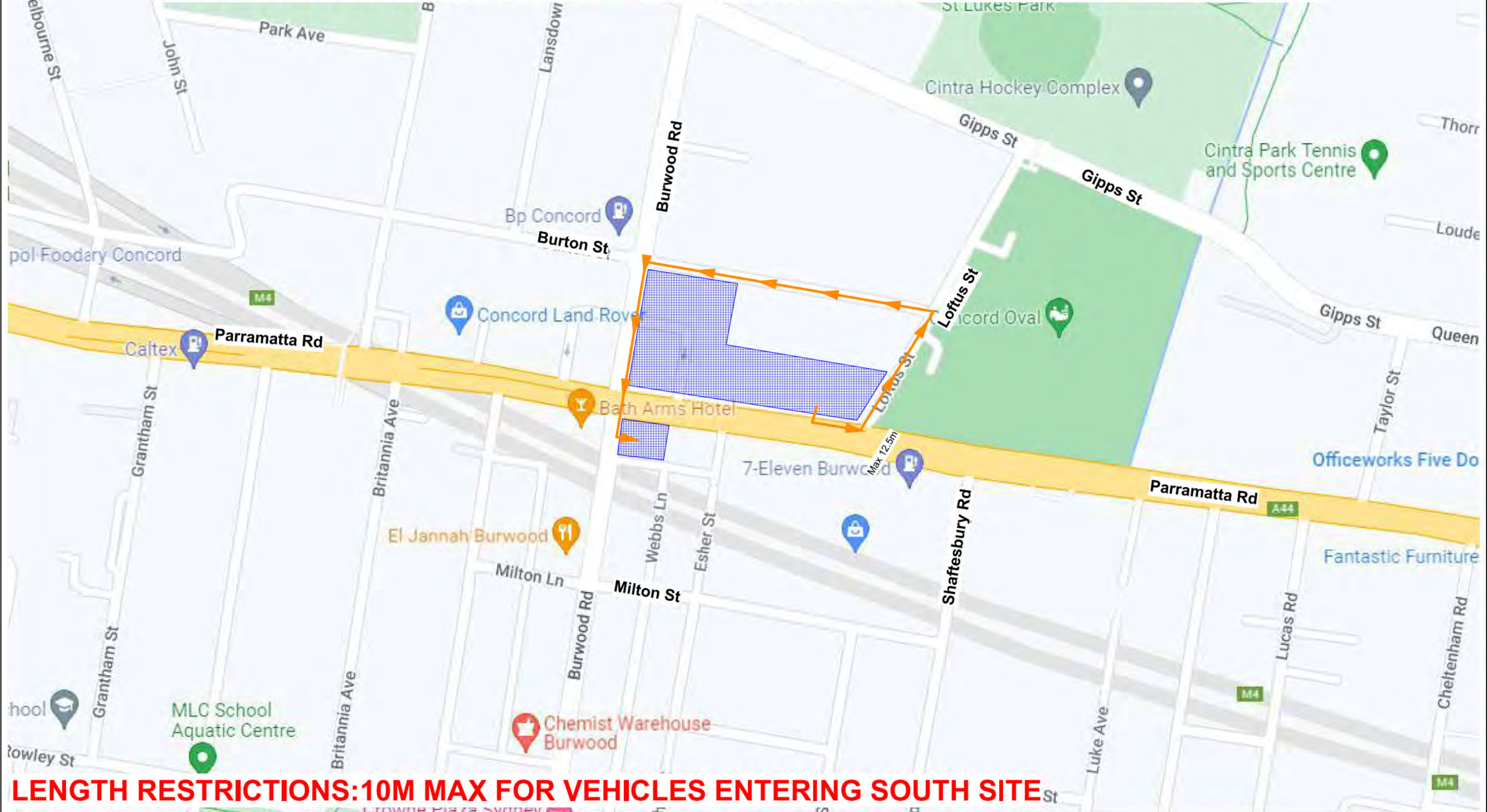
- Comments:**
- Drivers must be briefed on this VMP
 - Gatekeeper/s must be in position when gates are in use and the VMP requires it.
 - Drivers must adhere to Gatekeepers directions
 - Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
 - Use only approved haul routes



PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE	
LEGEND	
	Workzone
	Access
	Egress
	Signalised intersection
	Restricted movement



VEHICLE MOVEMENT PLAN - North site to South site movement



LENGTH RESTRICTIONS: 10M MAX FOR VEHICLES ENTERING SOUTH SITE

Date: 15/06/2023 **Location:** Burwood - Southern Site **Author name:**

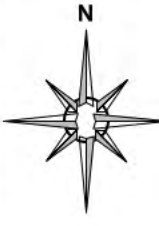
Comments:

- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes

PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Workzone		Signalised intersection
	Access		Restricted movement
	Egress		




VEHICLE MOVEMENT PLAN

Load Restriction - General Mass Limit (GML)

UHF: 12

DE-WATERING AREA:
Left in & out
Forward in & out
ONLY

SITE EGRESS

Traffic Control 'Gate Keeper' must be in place Any time there're truck movements at the dewatering area.
The Primary duty of the Traffic Controller is to maintain safe Pedestrian & vehicle interface. (Pedestrians MUST be provided priority)

DE-WATERING ACCESS ROUTE

SITE ACCESS

SOUTH SITE ACCESS ROUTE
MAX VEHICLE LENGTH 8.8m

FOOTPATH INSPECTION AND MAINTENANCE:

Gate Keepers MUST:

- Inspect footpaths at the start of each shift
- Inspect footpaths a minimum of once every 2hr
- Inspect the footpaths at the end of each shift

Any issues identified, such as:

- Tripping hazards
 - Dirt & debris
 - Degrading condition of the footpaths
 - Other issues that may lead to an increased risk to the public
- MUST be reported to the site supervisor immediately

The site Supervisor MUST assess and arrange rectification of the issue

Traffic Control 'Gate Keeper' must be in place Any time this gate is being used by heavy vehicles.
The Primary duty of the Traffic Controller is to maintain safe Pedestrian & vehicle interface.

Date: 05/04/2023 Location: North Strathfield Site

Author name:

Comments:

- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes



PROJECT:

SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

Rev02

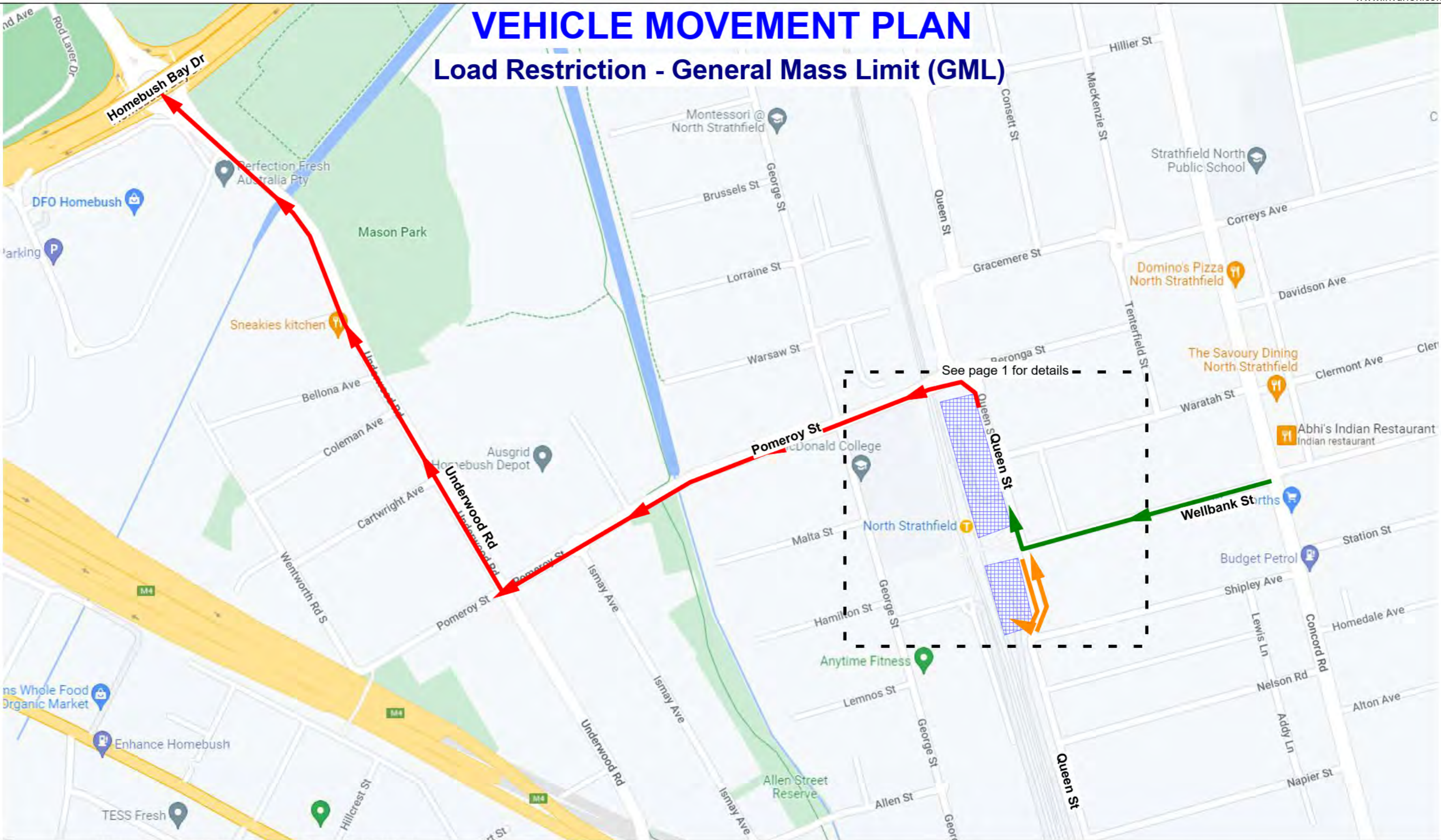
LEGEND

	Workzone		Signalised intersection
	Access		Other movement
	Egress		



VEHICLE MOVEMENT PLAN

Load Restriction - General Mass Limit (GML)



Date: 05/04/2023 **Location:** North Strathfield Site **Author name:**

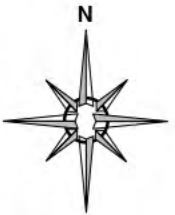
- Comments:**
- Drivers must be briefed on this VMP
 - Gatekeeper/s must be in position when gates are in use and the VMP requires it.
 - Drivers must adhere to Gatekeepers directions
 - Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
 - Use only approved haul routes



PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

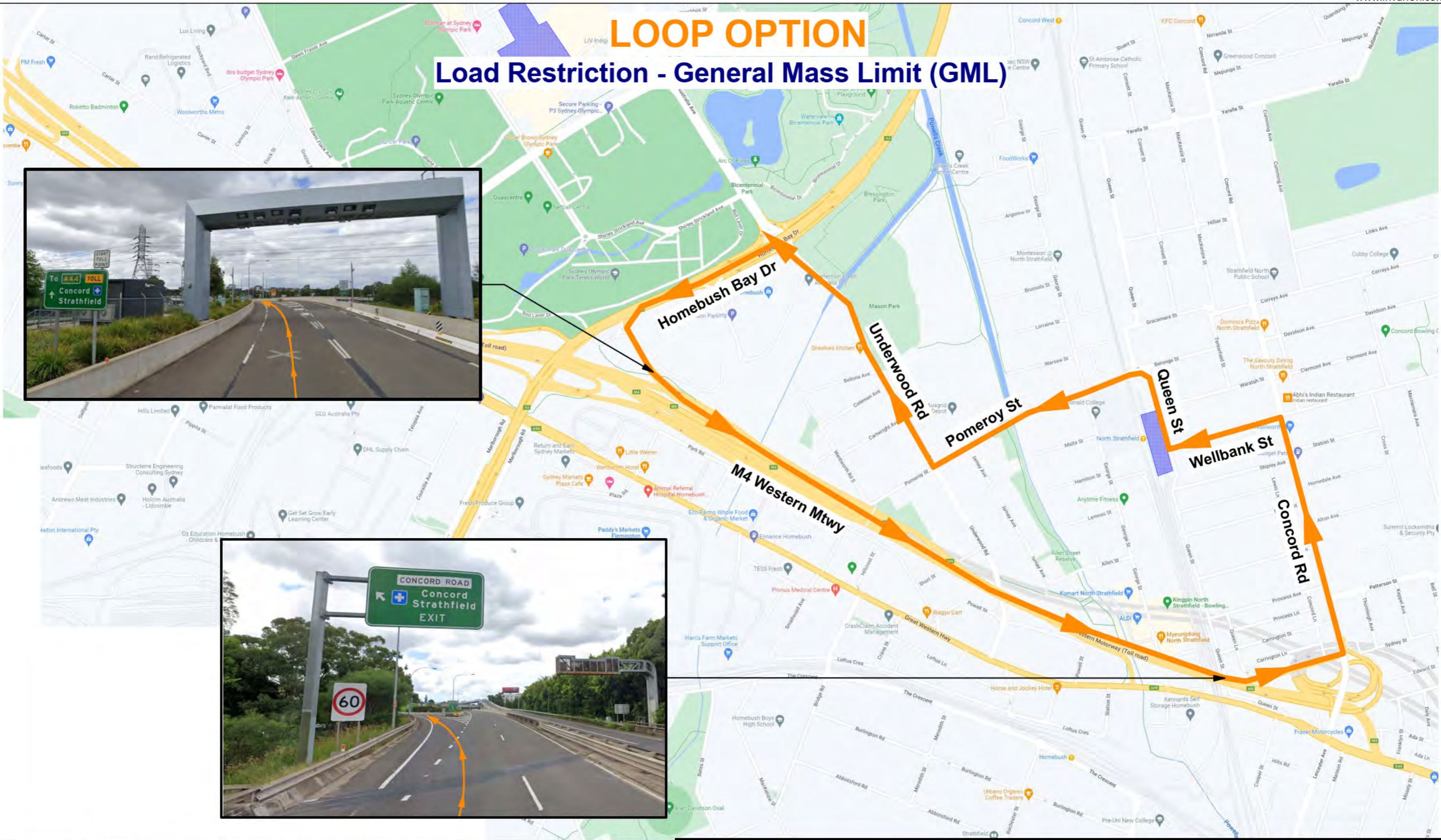
LEGEND

	Workzone		Signalised intersection
	Access		
	Egress		



LOOP OPTION






Load Restriction - General Mass Limit (GML)

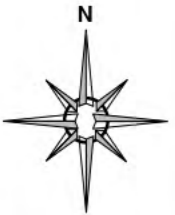


Date: 24/11/2022 **Location:** Sydney Olympic Park site **Author name:** [Redacted]

Comments:

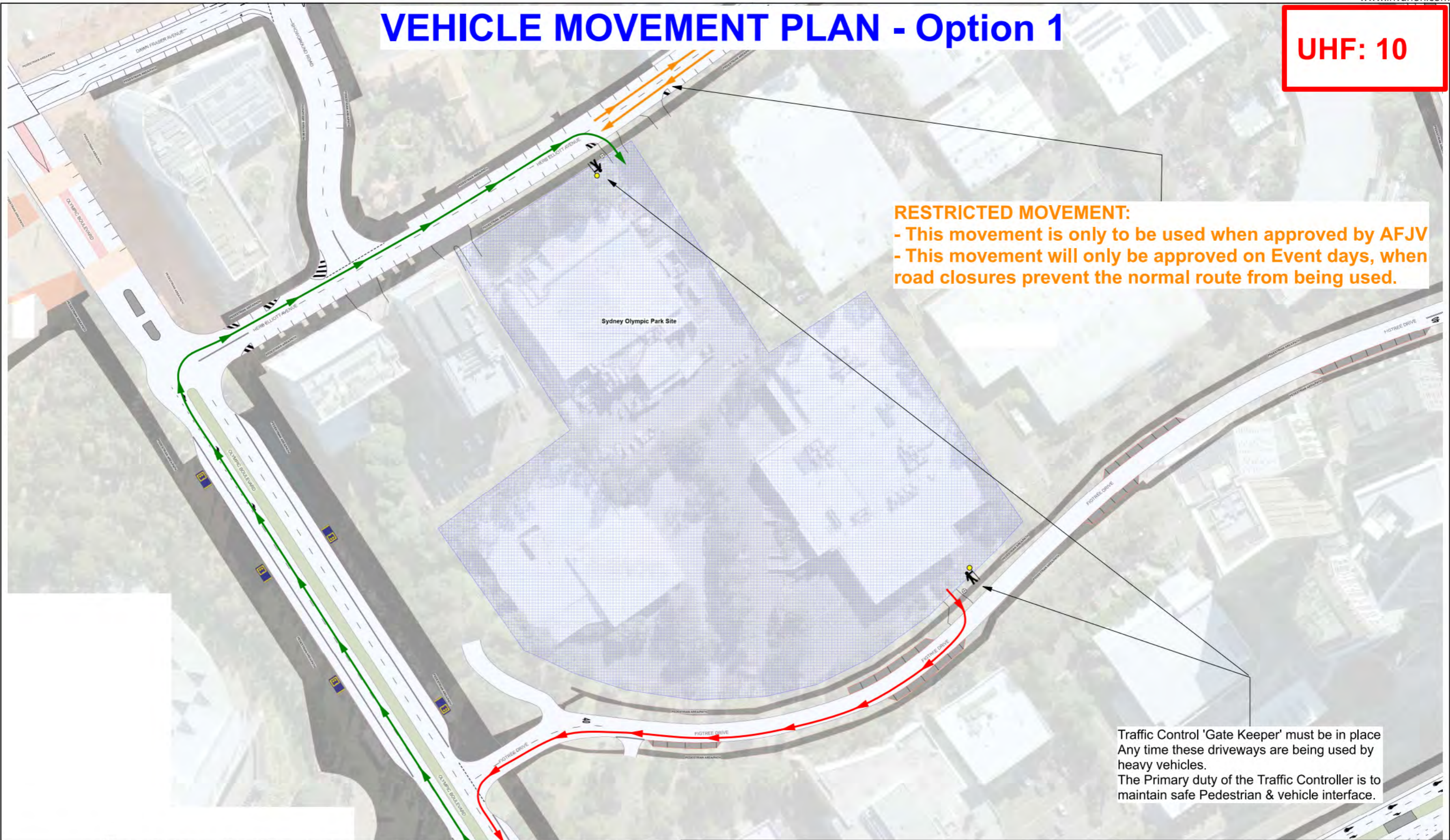
- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are on use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul; routes

PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE	
LEGEND	
	Workzone
	Access
	Egress
	Signalised intersection
	Loop option



VEHICLE MOVEMENT PLAN - Option 1

UHF: 10



RESTRICTED MOVEMENT:
 - This movement is only to be used when approved by AFJV
 - This movement will only be approved on Event days, when road closures prevent the normal route from being used.

Traffic Control 'Gate Keeper' must be in place Any time these driveways are being used by heavy vehicles. The Primary duty of the Traffic Controller is to maintain safe Pedestrian & vehicle interface.

Date: 14/10/2022 **Location:** Sydney Olympic Park site

Author name:

Comments:

- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are in use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haul routes

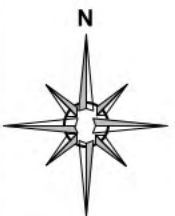


PROJECT:

SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

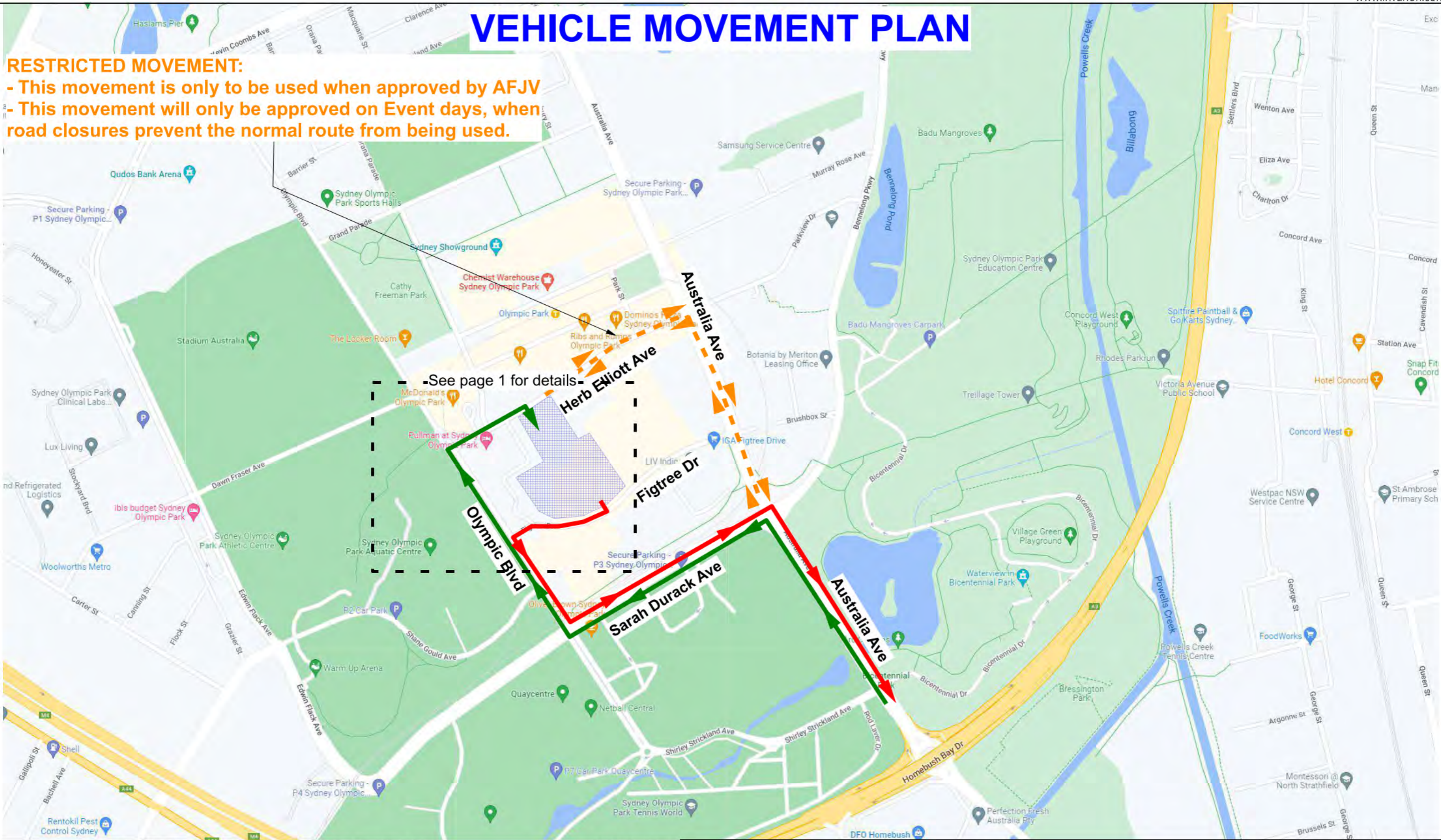
	Workzone		Signalised intersection
	Access		Restricted movement
	Egress		



VEHICLE MOVEMENT PLAN

RESTRICTED MOVEMENT:

- This movement is only to be used when approved by AFJV
- This movement will only be approved on Event days, when road closures prevent the normal route from being used.



-See page 1 for details-

Date: 14/10/2022 **Location:** Sydney Olympic Park site **Author name:**

- Comments:**
- Drivers must be briefed on this VMP
 - Gatekeeper/s must be in position when gates are on use and the VMP requires it.
 - Drivers must adhere to Gatekeepers directions
 - Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intension via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
 - Use only approved haulj routes



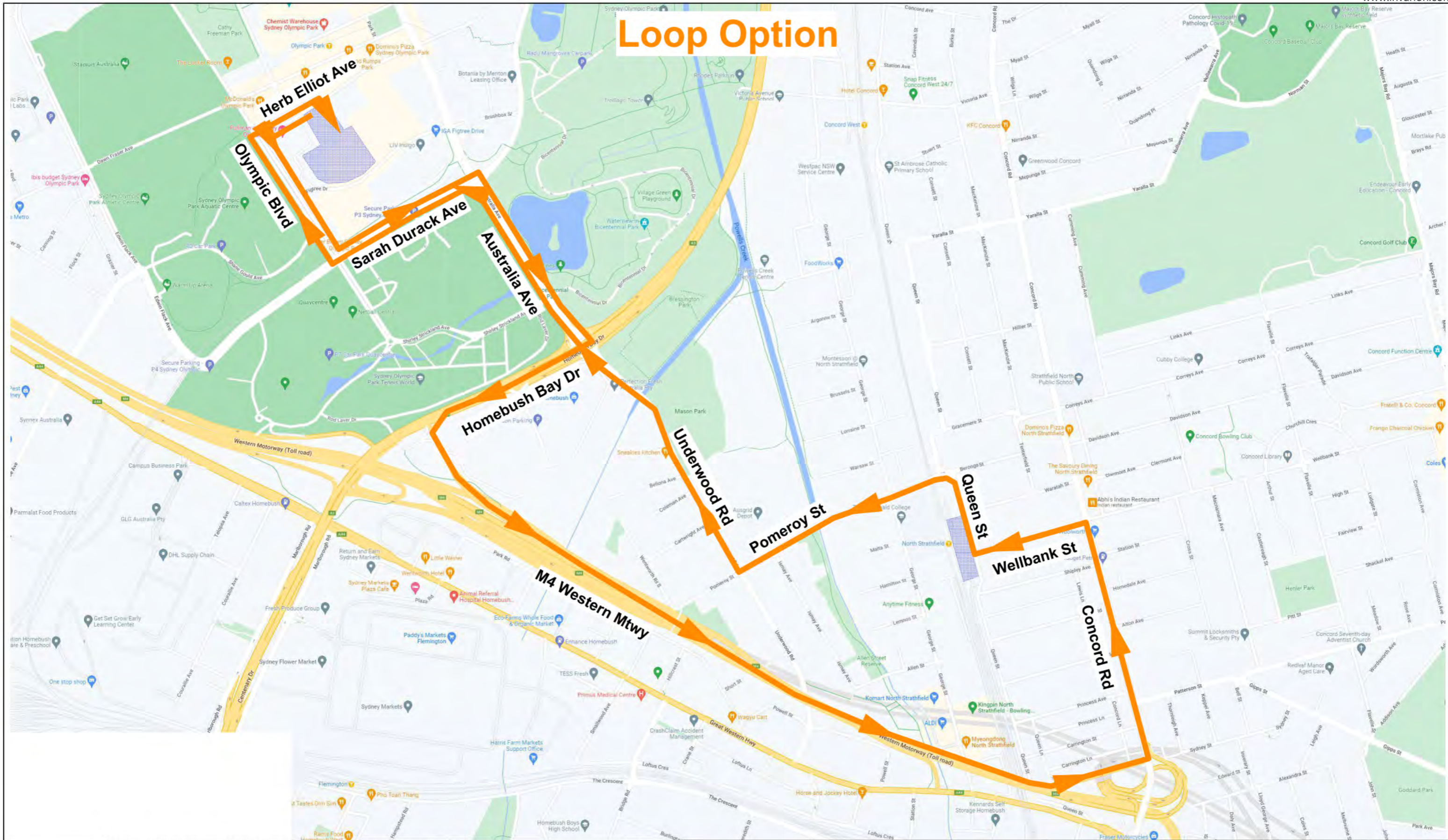
PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Workzone		Signalised intersection
	Access		Restricted movement
	Egress		



Loop Option



Date: 14/10/2022 **Location:** Sydney Olympic Park site

Author name



Comments:

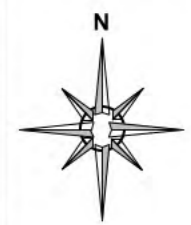
- Drivers must be briefed on this VMP
- Gatekeeper/s must be in position when gates are on use and the VMP requires it.
- Drivers must adhere to Gatekeepers directions
- Vehicles entering and exiting site must:
 1. Activate roof mounted beacons on approach
 2. radio intension via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.
- Use only approved haulj routes



PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

LEGEND

	Workzone		Signalised intersection
	Access		Loop option
	Egress		



6.3 WORKER PARKING MAPS

Park in designated areas only

Sydney Olympic Park - Construction Worker Parking

Not not park illegally

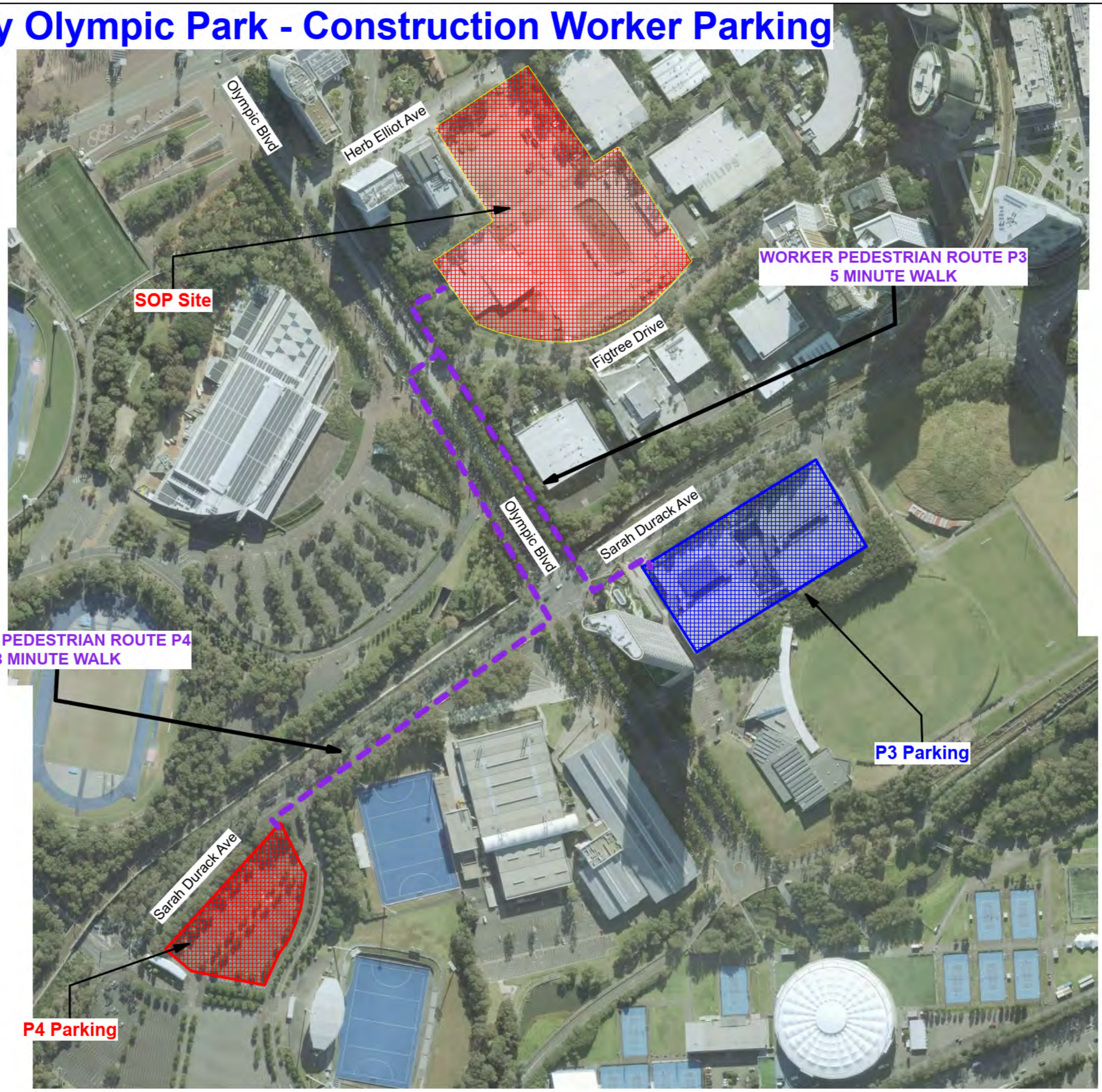
Do not block peoples driveways

Be respectful of local businesses and residents.

Do not park in areas that are not show on this map

WORKER PEDESTRIAN ROUTE P4
8 MINUTE WALK

WORKER PEDESTRIAN ROUTE P3
5 MINUTE WALK



SOP Site

P3 Parking

P4 Parking

Park in designated areas only

Strathfield - Construction Worker Parking

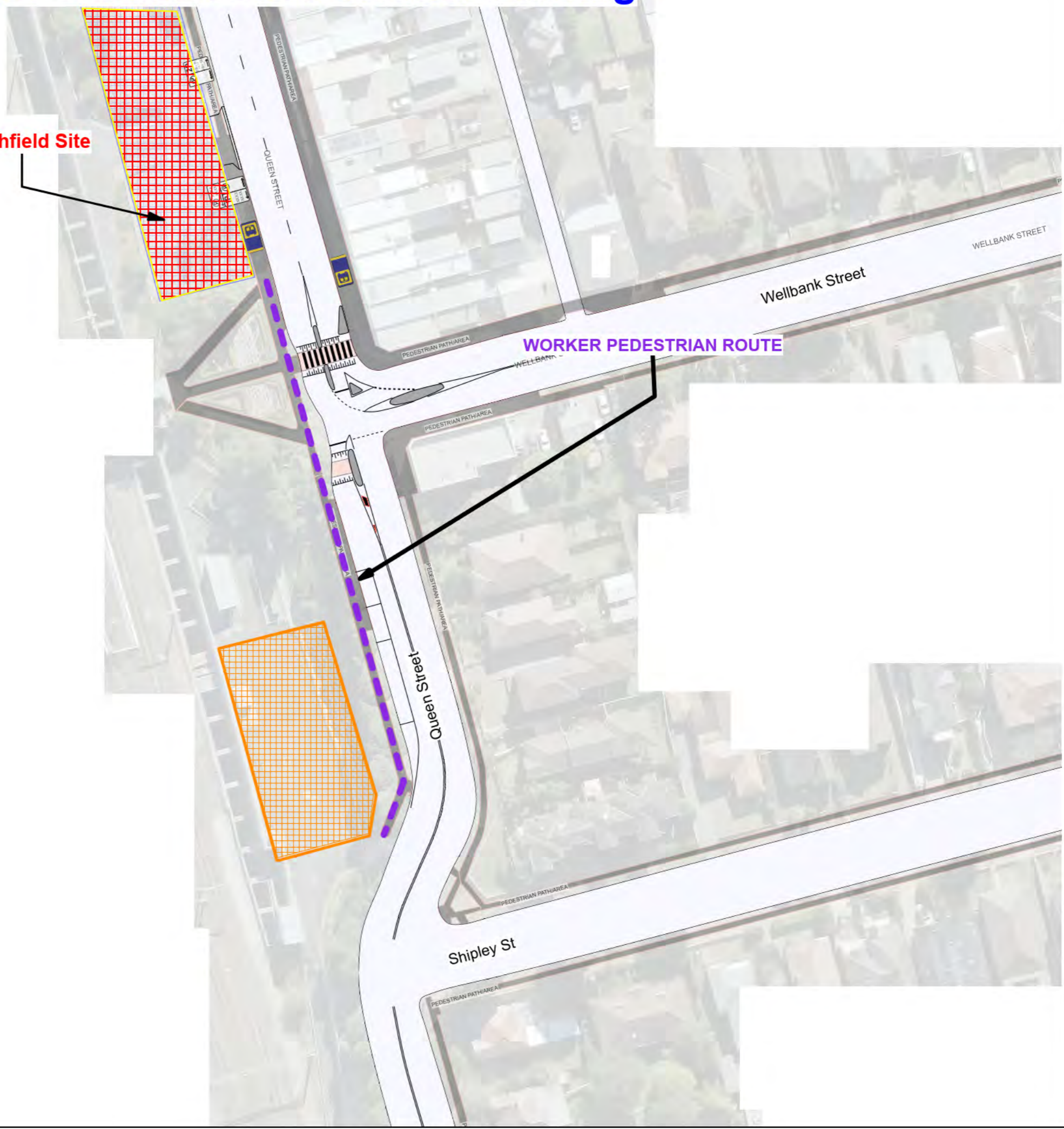
Do not park illegally

Do not block peoples driveways

Be respectful of local businesses and residents.

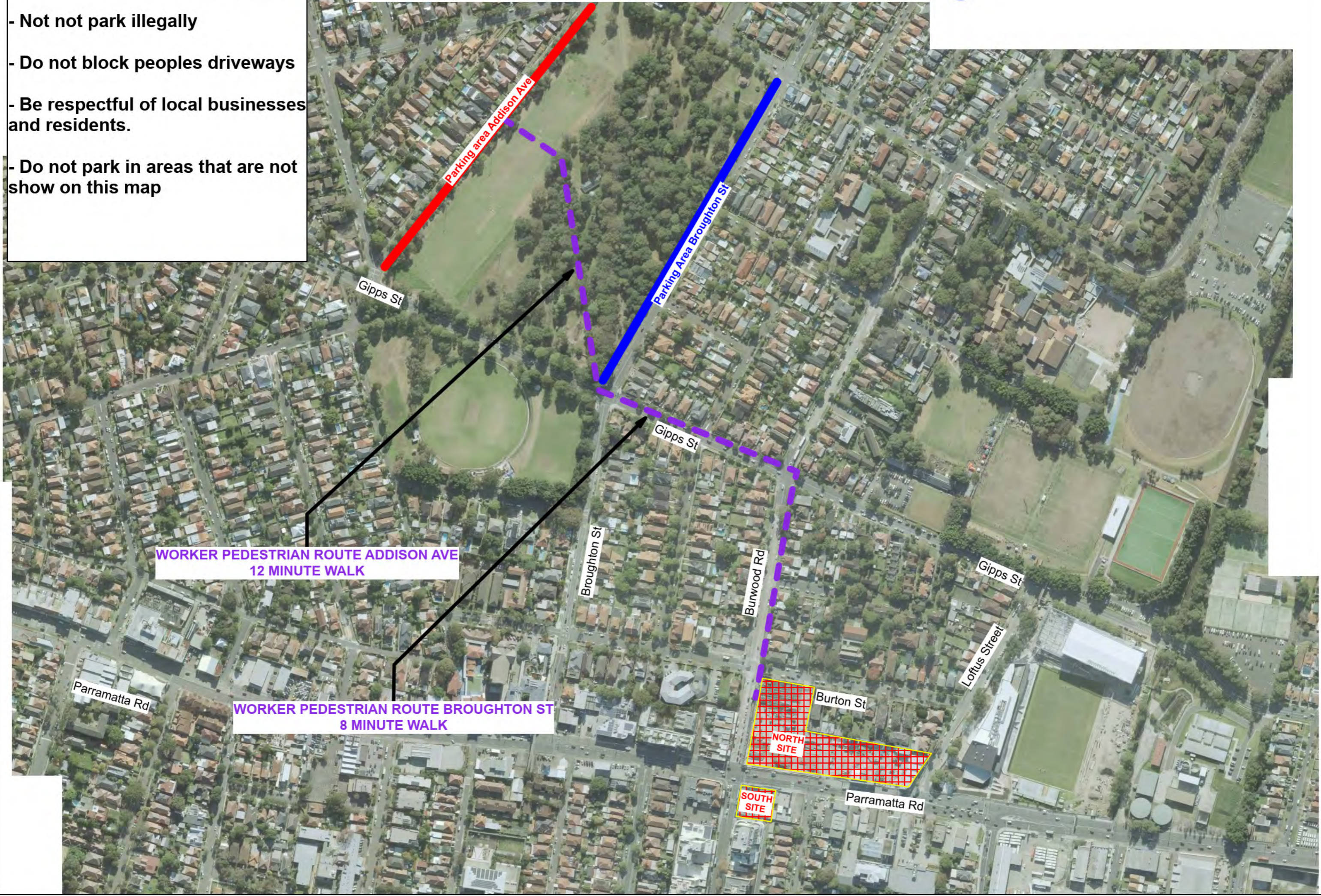
Strathfield Site

WORKER PEDESTRIAN ROUTE



Burwood - Construction Worker Parking

- Park in designated areas only
- Not not park illegally
- Do not block peoples driveways
- Be respectful of local businesses and residents.
- Do not park in areas that are not show on this map



Parking area Addison Ave

Parking Area Broughton St

WORKER PEDESTRIAN ROUTE ADDISON AVE
12 MINUTE WALK

WORKER PEDESTRIAN ROUTE BROUGHTON ST
8 MINUTE WALK

NORTH SITE

SOUTH SITE

Gipps St

Gipps St

Broughton St

Burwood Rd

Gipps St

Loftus Street

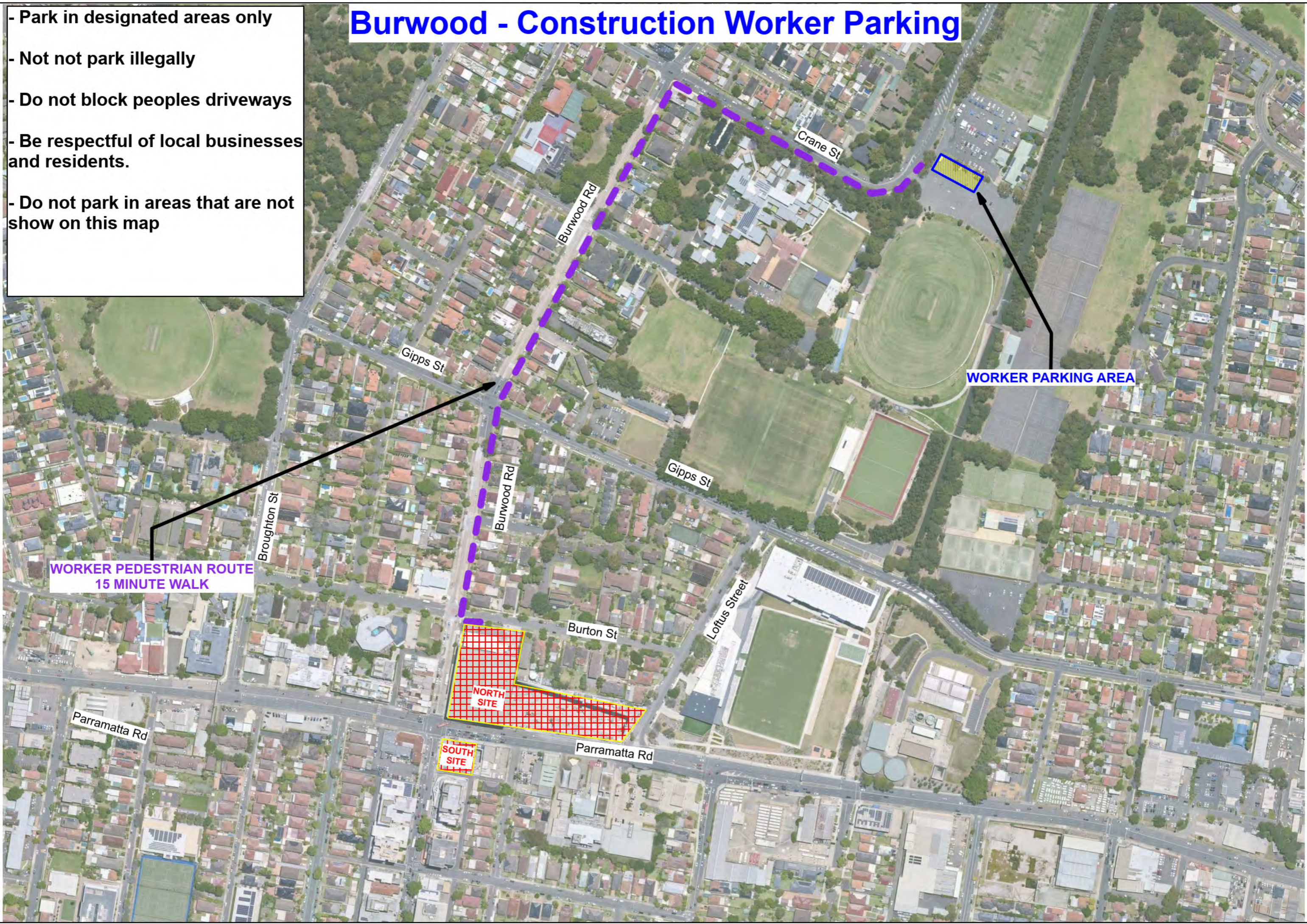
Burton St

Parramatta Rd

Parramatta Rd

Burwood - Construction Worker Parking

- Park in designated areas only
- Not not park illegally
- Do not block peoples driveways
- Be respectful of local businesses and residents.
- Do not park in areas that are not show on this map



WORKER PEDESTRIAN ROUTE
15 MINUTE WALK

NORTH SITE

SOUTH SITE

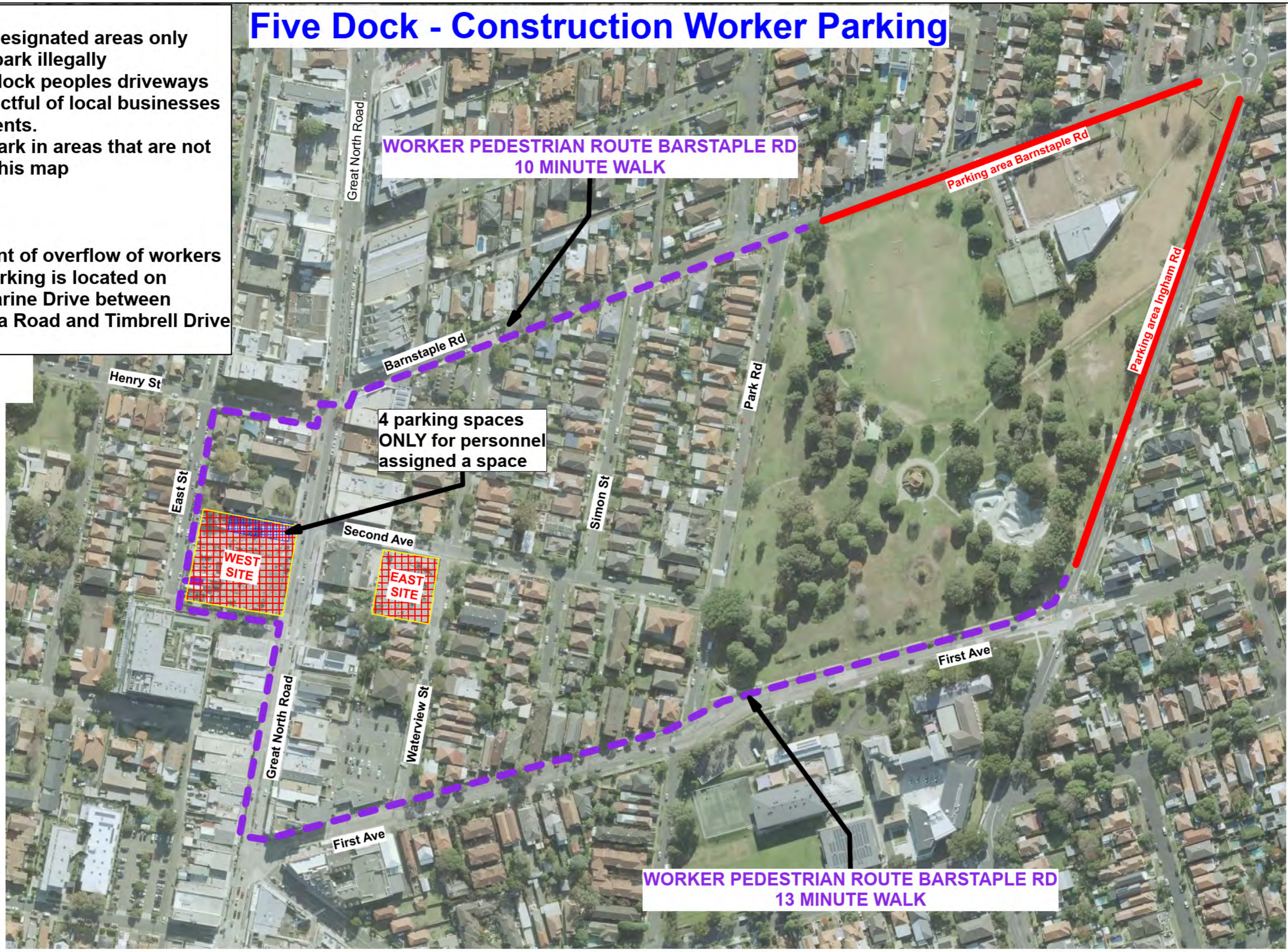
WORKER PARKING AREA

Five Dock - Construction Worker Parking

- Park in designated areas only
- Not not park illegally
- Do not block peoples driveways
- Be respectful of local businesses and residents.
- Do not park in areas that are not show on this map

Overflow:

In the event of overflow of workers excess parking is located on Hanley Marine Drive between Parramatta Road and Timbrell Drive



WORKER PEDESTRIAN ROUTE BARSTAPLE RD
10 MINUTE WALK

4 parking spaces
ONLY for personnel
assigned a space

WEST SITE

EAST SITE

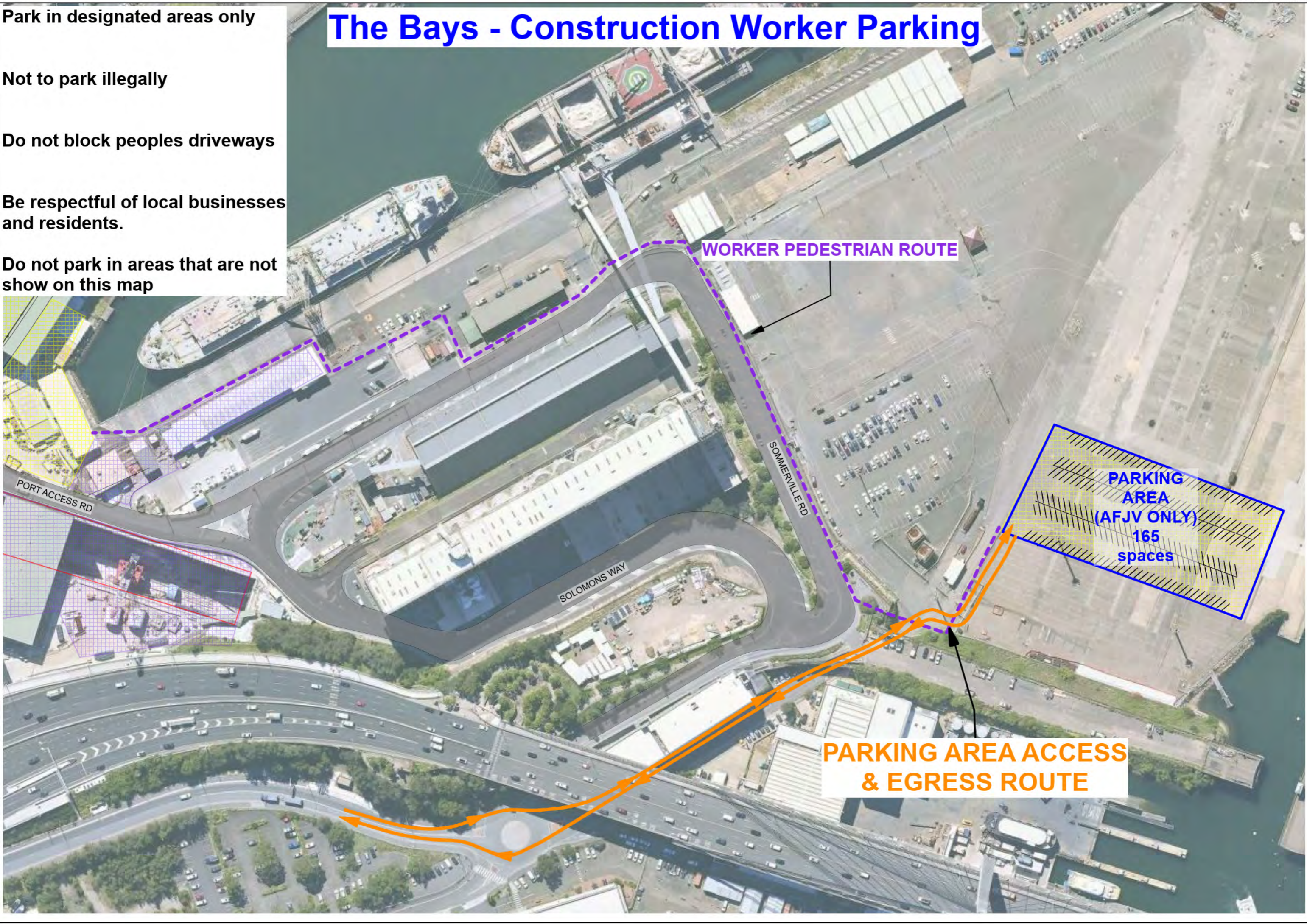
WORKER PEDESTRIAN ROUTE BARSTAPLE RD
13 MINUTE WALK

Parking area Barnstaple Rd

Parking area Ingham Rd

The Bays - Construction Worker Parking

- Park in designated areas only
- Not to park illegally
- Do not block peoples driveways
- Be respectful of local businesses and residents.
- Do not park in areas that are not show on this map



WORKER PEDESTRIAN ROUTE

PARKING AREA (AFJV ONLY) 165 spaces

PARKING AREA ACCESS & EGRESS ROUTE

PORT ACCESS RD

SOLOMONS WAY

SOMERVILLE RD

6.4 HEAVY VEHICLE TURN PATHS – MARSHALLING AREAS



OPEN FENCING TO EXIT MARSHALLING AREA
TRAFFIC CONTROL TO BE IN PLACE DURING USE

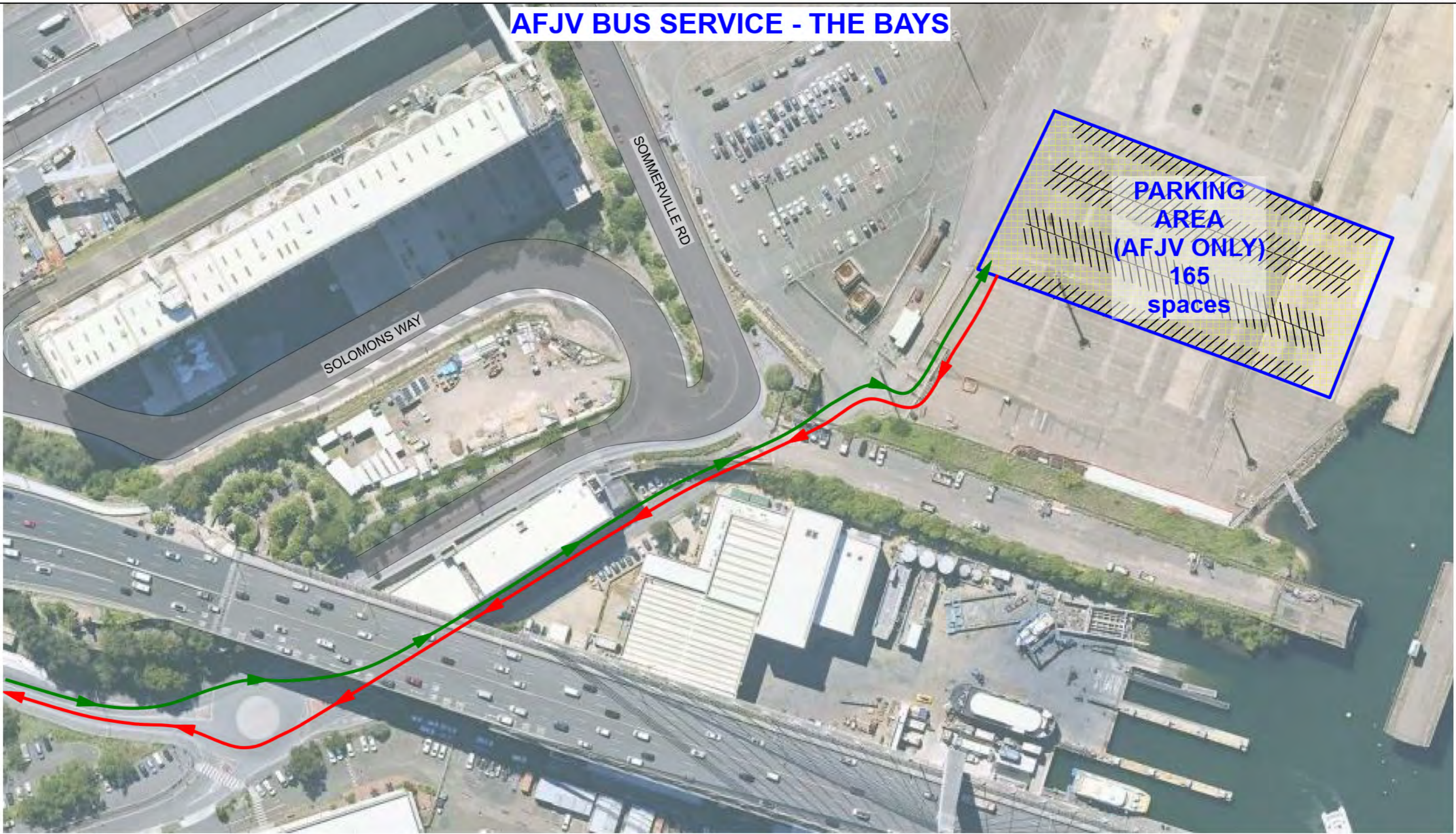
TRUCK MARSHALLING AREA FOR X2 19M TRUCKS

ORIGINAL DRAWING IN COLOUR



6.5 BUS SERVICES – VEHICLE MOVEMENT PLANS (VMP)

AFJV BUS SERVICE - THE BAYS



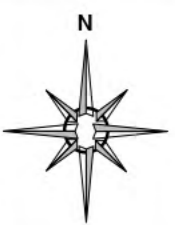
Date: 12/12/2023 **Location:** PROJECT WIDE **Author name:**

- Comments:**
- Bus driver must be briefed on this VMP
 - Bus driver must adhere to gate keeper instructions
 - Bus driver must
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.

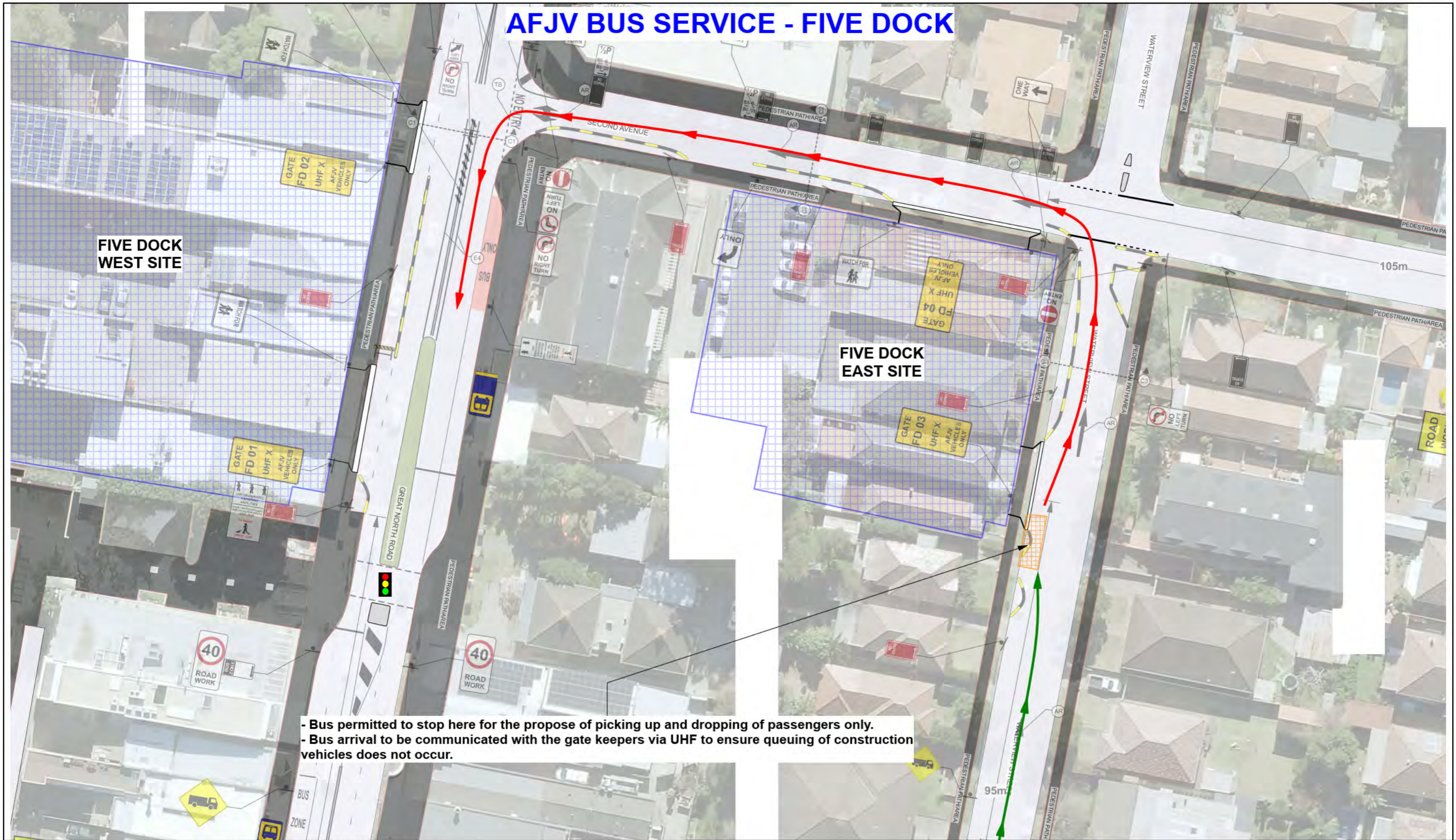


PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE Rev01

LEGEND	
	Access
	Egress



AFJV BUS SERVICE - FIVE DOCK



- Bus permitted to stop here for the propose of picking up and dropping of passengers only.
 - Bus arrival to be communicated with the gate keepers via UHF to ensure queuing of construction vehicles does not occur.

Date: 12/12/2023 **Location:** PROJECT WIDE **Author name:**

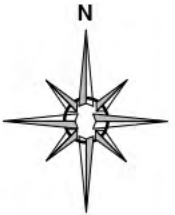
Comments:

- Bus driver must be briefed on this VMP
- Bus driver must adhere to gate keeper instructions
- Bus driver must
 1. Activate roof mounted beacons on approach
 2. radio intension via UHF
 3. Indicate intensions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.

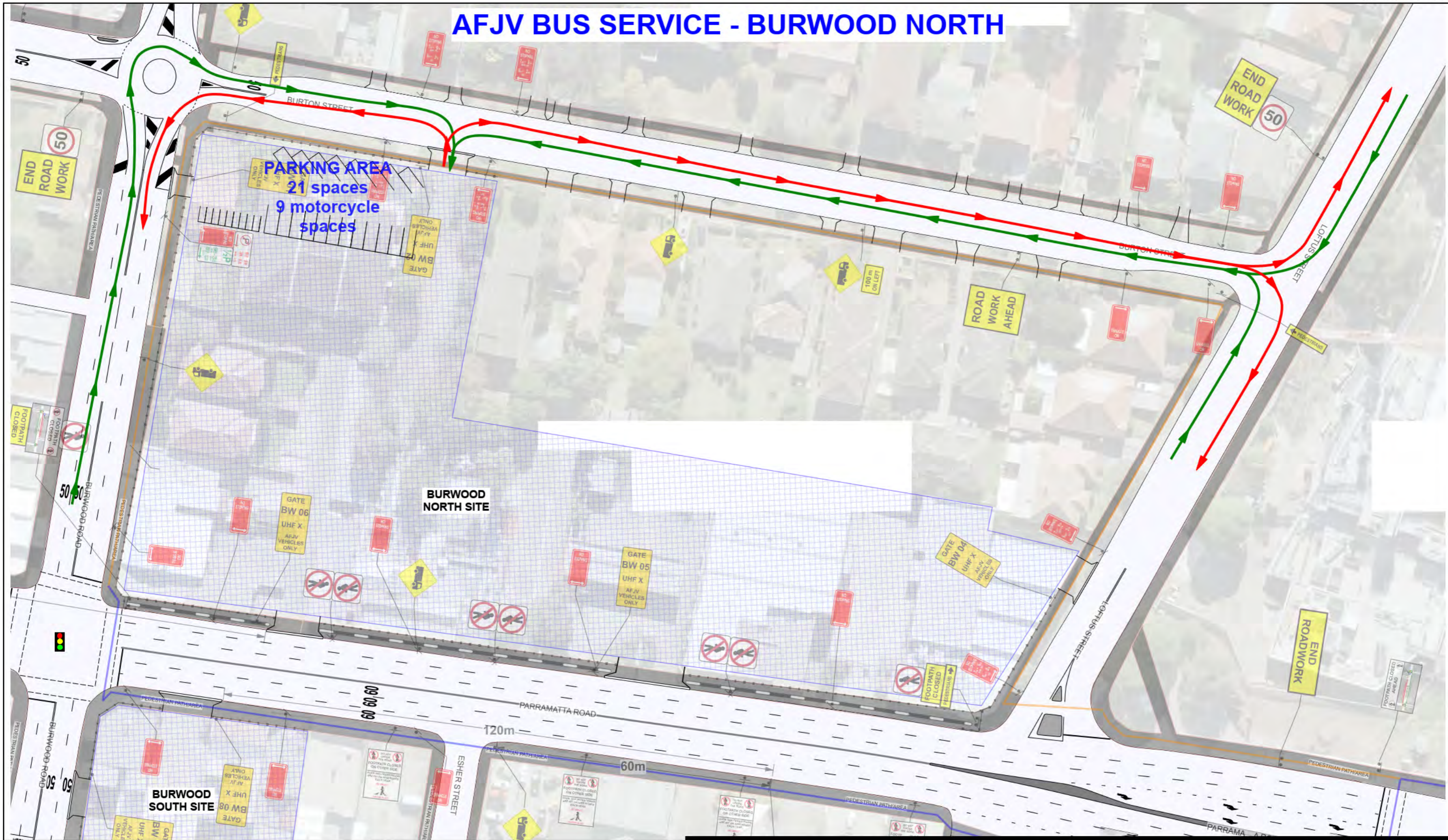
PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE **Rev01**

LEGEND

-  Access
-  Egress



AFJV BUS SERVICE - BURWOOD NORTH



Date: 12/12/2023 Location: PROJECT WIDE Author name:

Comments:

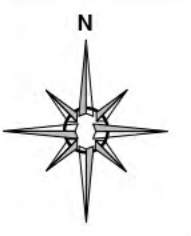
- Bus driver must be briefed on this VMP
- Bus driver must adhere to gate keeper instructions
- Bus driver must
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.



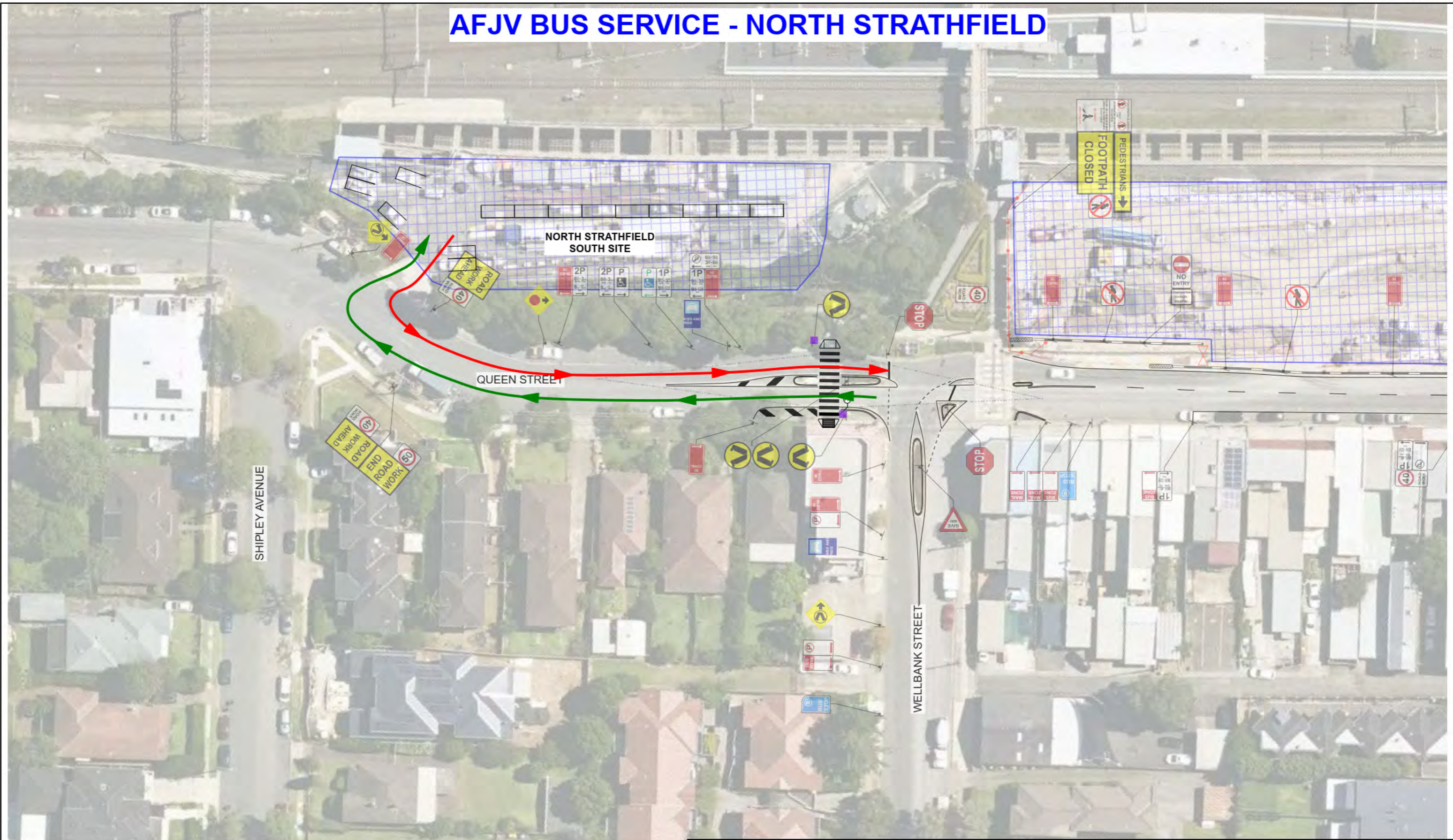
PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE Rev01

LEGEND

- Access
- Egress



AFJV BUS SERVICE - NORTH STRATHFIELD



Date: 12/12/2023 **Location:** PROJECT WIDE **Author name:**

Comments:

- Bus driver must be briefed on this VMP
- Bus driver must adhere to gate keeper instructions
- Bus driver must
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.



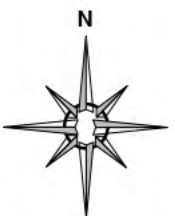
PROJECT:

SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE

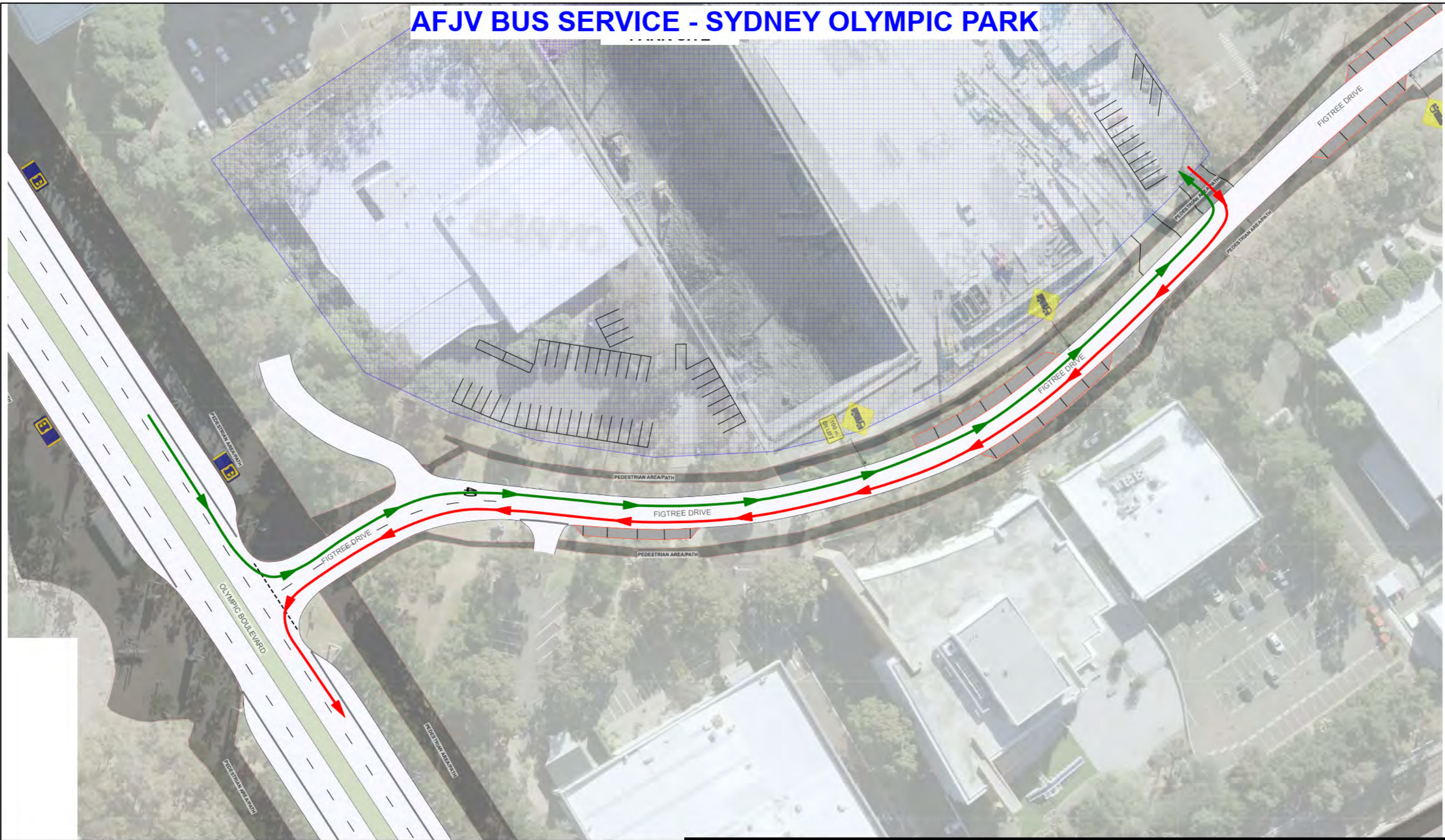
Rev01

LEGEND

- Access
- Egress



AFJV BUS SERVICE - SYDNEY OLYMPIC PARK



Date: 12/12/2023 **Location:** PROJECT WIDE **Author name:**

Comments:

- Bus driver must be briefed on this VMP
- Bus driver must adhere to gate keeper instructions
- Bus driver must
 1. Activate roof mounted beacons on approach
 2. radio intention via UHF
 3. Indicate intentions
 4. Turn into/out of site
 5. Exit with caution, ensuring the safety of pedestrian and other road users
 6. Disable roof mounted beacons after egress and speed has reached normal traffic flow.
 7. follow all road rules and speed limits.



PROJECT: SYDNEY METRO WEST - CENTRAL TUNNEL PACKAGE Rev01

LEGEND

- Access
- Egress

